





This Warragul Town Centre Master Plan was prepared by Hansen Partnership in association with Cardno Grogan Richards and Tim Nott Economics for Baw Baw Shire Council.

This Warragul Town Centre Master Plan is a shared initiative of the State Victorian Government and Baw Baw Shire Council.

May 2011

table of contents

1	introduction	4		8.2.4 drinking fountains	20
2	smith street & palmerston street 'retail heart'	7	8.3	plant species	20
	vision	7		8.3.1 street trees	20
	actions	7		8.3.2 garden beds	20
	probable cost	7		8.3.3 rain gardens	20
3	smith street green spine	10	9	development sites	21
	vision	10	9.1	triangle precinct	30
	actions	10		vision	30
	probable cost	10		guidelines	30
4	trinca lane	12	9.2	william square precinct	32
	vision	12		vision	32
	actions	12		guidelines	32
	probable cost	12		land use	32
5	laneways	14		parking and access	32
	vision	14		built form	32
	actions	14	9.3	butter factory precinct	34
6	civic precinct	16		vision	34
	vision	16		guidelines	34
	guiding principles	16		land use	34
7	albert & gladstone streets	18		parking and access	34
	vision	18		buildings	34
	guidelines	18	9.4	gateway sites	36
8	public domain materials guidelines	19		vision	36
8.1	pavements	19		southern gateway	36
	8.1.1 pedestrian pavements - general	19		western gateway	38
	8.1.2 pedestrian pavements - permeable surfaces	19	10	traffic and transport assessment	40
8.2	street furniture	19	11	implementation	41
	8.2.1 bench seats	19	10	appendices	43
	8.2.2 rubbish bins	19		appendix 1: probable costs	44
	8.2.3 bicycle rails	19			

table of figures

Figure 1	urban design framework plan	5
Figure 2	master plan precinct context map	6
Figure 3	smith street / palmerston street 1	8
Figure 4	smith street / palmerston street 2	9
Figure 5	smith street green spine	11
Figure 6	trinca lane	13
Figure 7	laneways	15
Figure 8	civic precinct concept	17
Figure 9	future development precincts	18
Figure 10	development sites locality map	19
Figure 11	parapet street wall treatments: typical cross section	19
Figure 12	floor plan basement level	20
Figure 13	floor plan ground level	21
Figure 14	floor plan first level	22
Figure 15	floor plan second level	23
Figure 16	floor plan third level	24
Figure 17	floor plan fourth level	25
Figure 18	indicative development vertical and horizontal detail	26
Figure 19	triangle precinct section	27
Figure 20	william square precinct sections	29
Figure 21	butter factory precinct sections	31
Figure 22	southern gateway section	33
Figure 23	southern gateway view from the corner of howitt and alfred streets	36
Figure 24	southern gateway section	37
Figure 25	view from queen street and trinca lane intersection of the corner feature form	38
Figure 26	aerial view of the gateway from queen st (east)	38
Figure 27	western gateway section	39

1 introduction

The Warragul Town Centre Urban Design Framework (UDF) was adopted by Baw Baw Shire on 12 May 2010. Council commenced preparation of the Warragul Town Centre Masterplan following the adoption of the UDF. The Masterplan focuses on 9 key areas within the Warragul Town Centre, including public works projects to enhance the streetscape environment, and development feasibility and guidance for a number of key privately owned development sites.

The Warragul Town Centre Urban Design Framework (UDF), station precinct master plan and town centre master plan are dependent documents which provide a long term framework for the retail area of Warragul, to accomodate anticipated growth as predicted in the 'Warragul & Drouin Growth Management Strategy 2005'.

The UDF states the following vision for the town centre:

“Warragul will develop a compact Town Centre that supports a thriving mix of business and residential activity whilst at all times respecting its rural origins and notable heritage assets. The centre will showcase a series of high amenity pedestrian areas and regenerate its traditional link with an enhanced Station precinct. It will also support a series of emerging educational, recreational and commercial facilities that will help to define the town’s ‘green spine’ and centralised community spaces.”

The UDF sets out a 30 year plan for the development and growth of the town centre which is underpinned by the following 14 principles:

- Principle 1:** create a 'green link' through the Town Centre
- Principle 2:** integrate new development with the existing retail core
- Principle 3:** enhance the 'historic precinct' (Queen Street)
- Principle 4:** prioritise pedestrian amenity
- Principle 5:** create a public transport interchange
- Principle 6:** promote education within the Station Precinct

- Principle 7:** establish a clear road hierarchy
- Principle 8:** identify appropriate development opportunities
- Principle 9:** protect important viewsheds to rural surrounds
- Principle 10:** establish a bulky goods precinct
- Principle 11:** strengthen sense of arrival at gateways
- Principle 12:** 'green' the Town Centre
- Principle 13:** define central business district boundary
- Principle 14:** create a centrally located community hub

The Warragul Town Centre Master Plan aims include:

- Produce detailed development guidance in response to the vision for future growth and development defined by the Warragul town centre urban design framework
- Provide a strategic context and advice for the development of specific sites across the town centre in accordance with the UDF's vision
- Revitalise the Warragul town centre through improved infrastructure, high quality urban design and public spaces
- Improve access to the town centre including by sustainable transport modes and encourage the integration of all transport modes
- Encourage the integration of residential development at higher densities
- Improve access to services
- Facilitate appropriate private investment and development through greater planning certainty; and stimulate greater local employment opportunities.

The focus of the master plan is on the following 9 key areas:

- Smith Street / Palmerston Street
- Smith Street Green Spine
- Trinca Lane
- Laneways

- Albert and Gladstone Street
- Triangle Precinct
- William Square Precinct
- Butter Factory Precinct
- Gateway Sites

The locations of these areas are identified in figure 2.

Whilst the master plan is a conceptual document, appropriate staging should be incorporated into the detailed consideration and planning of recommended works. Alternative plans have been included which can be provided in the interim where dependencies exist.

The following chapters outline the master plan vision and development directions for each of the areas.

warragul town centre urban design framework

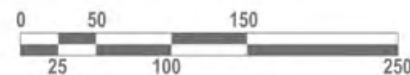
urban design framework

legend

- urban design framework study area
- key retail / commercial
- large format retail
- offices / mixed use
- key educational facilities/ churches
- mixed use infill development
- bulky goods / main road sales
- civic, community and medical
- residential consolidation / consulting suites
- key landmarks
- potential landmark sites
- railway line
- reinforce main roads
- secondary road
- local streets
- round-a-bout intersections
- signalised intersections
- pedestrian link (service access available)
- service ways
- key pedestrian priority places
- key car parking cells
- distinctive main street planting
- distinctive street planting
- established heritage trees
- key public open spaces / recreation areas



Figure 1 warragul town centre urban design framework plan



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1:5000@A3
Date: 28.04.10
Revision: C

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warragul town centre
master plan
precinct context map

- legend
- smith street / palmerston st upgrades
 - smith street 'green spine'
 - gateway sites
 - trinca lane
 - civic precinct
 - albert and gladstone streets
 - triangle precinct
 - william square precinct
 - butter factory precinct
 - laneways



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2 smith street & palmerston street ‘retail heart’

background

The Warragul Town Centre Urban Design Framework identified the need to establish central community spaces in the town centre. The ‘heart’ of the centre lies around the Palmerston Street elbow and the Southern end of Smith Street.

vision

The Smith Street and Palmerston Street precinct seeks to reinvigorate the ‘Retail Heart’ of Warragul by improving the pedestrian experience within central Warragul.

A reconfiguration of vehicular movement to one-way eastbound along Palmerston Street (between Victoria Street and Smith Street) and one-way southbound along Smith Street between Palmerston Street and Victoria Street is proposed. This reconfiguration provides significant opportunities for a reduction in road carriageway widths and expansion of footpaths, allowing the establishment of open community spaces, generous pedestrian boulevards, improved outdoor retail and dining experiences and enhanced street tree and garden bed landscaping. All of these measures are aimed at improving the usability, amenity and attractiveness of the precinct, as a means of encouraging more people to visit, occupy and enjoy the public areas of central Warragul.

Importantly, the vision seeks to achieve these outcomes without any reduction in the number of on-street parking spaces presently available within these sections of Smith and Palmerston Streets.

The Smith Street and Palmerston Street ‘Retail Heart’ contributes to the achievement of the following Urban Design Framework Principles:

- **Principle 1** – Create a ‘green link’ through the town centre
- **Principle 4** – Prioritise pedestrian amenity
- **Principle 7** – Establish a clear road hierarchy
- **Principle 12** – ‘Green’ the town centre
- **Principle 14** – Create a centrally located community hub

actions

- Reduce vehicular traffic along both Palmerston and Smith Streets by reconfiguring them to provide one-way traffic.
- A bus stop should be conveniently located in the city centre to provide for improved public transport access to the Retail Heart of Warragul. Possible locations of the bus stop will be monitored as the plan is implemented and will be subject to further discussion with stakeholders as demand increases and parking supply improves in the city centre area to offset any potential loss of on street parking.
- Work closely with public transport providers to reassess the bus routes through central Warragul as part of the reconfiguration of the roadways.
- Provide a pedestrian/vehicular shared zone within the ‘elbow’ of Palmerston Street, configured in such a way that it can be utilised for large community events in association with temporary street closures.
- Explore opportunities to provide a new free-standing Community Kiosk (in place of existing) within the Palmerston Street shared zone.
- Retain and protect the existing heritage-listed Elm tree on the south side of Palmerston Street and incorporate new landscaping treatments to emphasise its importance.
- Reconfigure the south side of Palmerston Street to provide a wide pedestrian promenade which can be utilised for outdoor dining and retail activities in an area which has good solar access and is relatively sheltered from colder prevailing winds.
- Explore opportunities for the incorporation of public art within Palmerston Street.
- Provide pavement upgrades along the length of Palmerston and Smith Streets, to improve both visual quality and ensure adequate accessibility for all pedestrians, including those with limited mobility.
- Incorporate new, tall clear-trunked street trees within Smith Street, in associated with pavement expansion, to establish a

strong vegetated avenue and reinforce the idea of the “green spine” along Smith Street as envisaged through the Urban Design Framework.

- Retain existing angled parking on the east side of Smith Street.
- Reconfigure car parking on the west side of Smith Street – in association with footpath expansion and kerb realignment – to provide parallel parking. An alternative option is to provide angle parking to both sides, in conjunction with one-way vehicle movement.
- Establish a broad, shaded pedestrian promenade along the west side of Smith Street, with opportunities for outdoor dining and retail activities.
- Close and reconfigure the existing service lane which connects Victoria Street and Smith Street immediately north of the War Memorial. Provide pavement upgrades and new street tree planting to establish this space as an open and easily-accessible community gathering space.
- Retain the ceremonial lawn and garden beds around the War Memorial.
- Reconfigure the intersection of Smith Street and Victoria Streets to provide through-traffic priority on Victoria Street and provide a pedestrian crossing on Smith Street to improve access to the war Memorial and adjacent retail areas.
- Retain existing mid-block pedestrian crossing on Smith Street, between Palmerston and Williams Street.

probable cost

Smith Street (between Victoria Street and Palmerston Street)
Approximate Total Excluding GST: \$779,700.00

Palmerston Street (Between Victoria Street and Smith Street)
Approximate Total Excluding GST: \$711,275.00

For a breakdown of costings please refer to Appendix 1 on page 43



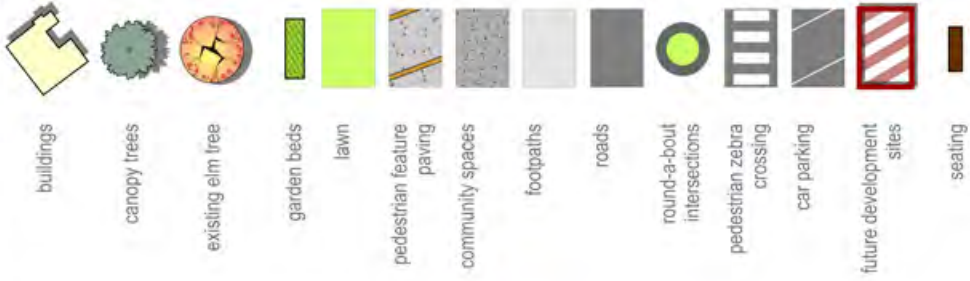
smith st / palmerston st

the smith street and palmerston street project seeks to reinvigorate the retail centre of warragul by improving the pedestrian experience and reducing vehicular traffic.

to realise this, this concept proposes a reconfiguration of vehicular movement to a one way arrangement along part of palmerston street (between victoria street and smith street) and smith street (between victoria street and palmerston street). this will then allow for the creation of open community spaces, generous pedestrian boulevards, outdoor retail and dining experiences and extensive landscaping and street tree planting, improving the usability and attractiveness of this area

the reconfigured road and parking arrangements result in no loss of car parking spaces.

legend



actions

- 1 reduce vehicular traffic along palmerston and smith streets by reconfiguring them to offer one way traffic movement only and bus interchange
- 2 retain angle parking
- 3 provide a pedestrian and vehicular shared zone within the palmerston street elbow. this open space can be used for large community events by closing the street off to traffic
- 4 incorporate a freestanding kiosk café within the palmerston street shared zone/
- 5 retain the existing heritage listed elm tree as a dominant visual feature of palmerston street
- 6 reconfigure car parking along palmerston street to provide a wide pedestrian promenade along the southern side which can be used for outdoor dining and retail
- 7 incorporate public art as a defining visual element of the palmerston elbow from victoria street
- 8 provide pavement upgrades throughout palmerston and smith street
- 9 incorporate new street tree planting to create strong vegetated avenues and establish the 'green spine' along smith street
- 10 retain the existing angled car parking on the east side of smith street
- 11 rearrange the car parking on the west side of smith street to a parallel arrangement
- 12 establish a wide pedestrian boulevard with space for outdoor dining and retail along the west side of smith street
- 13 close the existing road and convert it into an open community gathering space
- 14 retain the ceremonial lawn around the cenotaph and establish trees behind it to provide an attractive backdrop
- 15 reconfigure the smith street and victoria street intersection and provide a pedestrian priority crossing
- 16 retention of existing taxi rank



smith st alternative parking option
provide angle parking in lieu of parallel parking



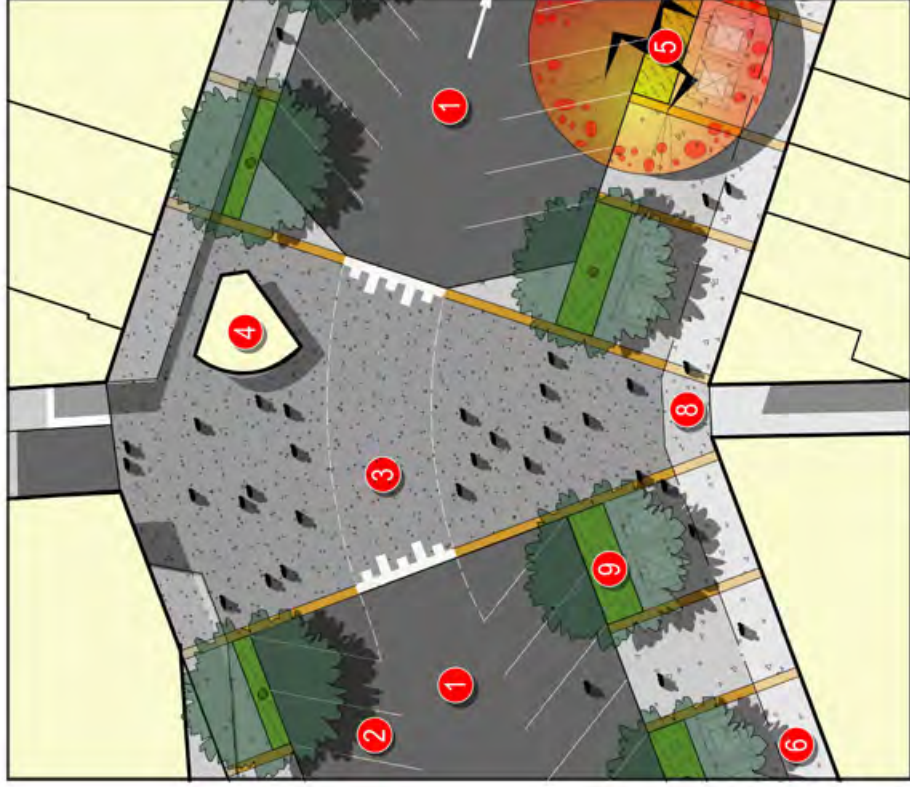
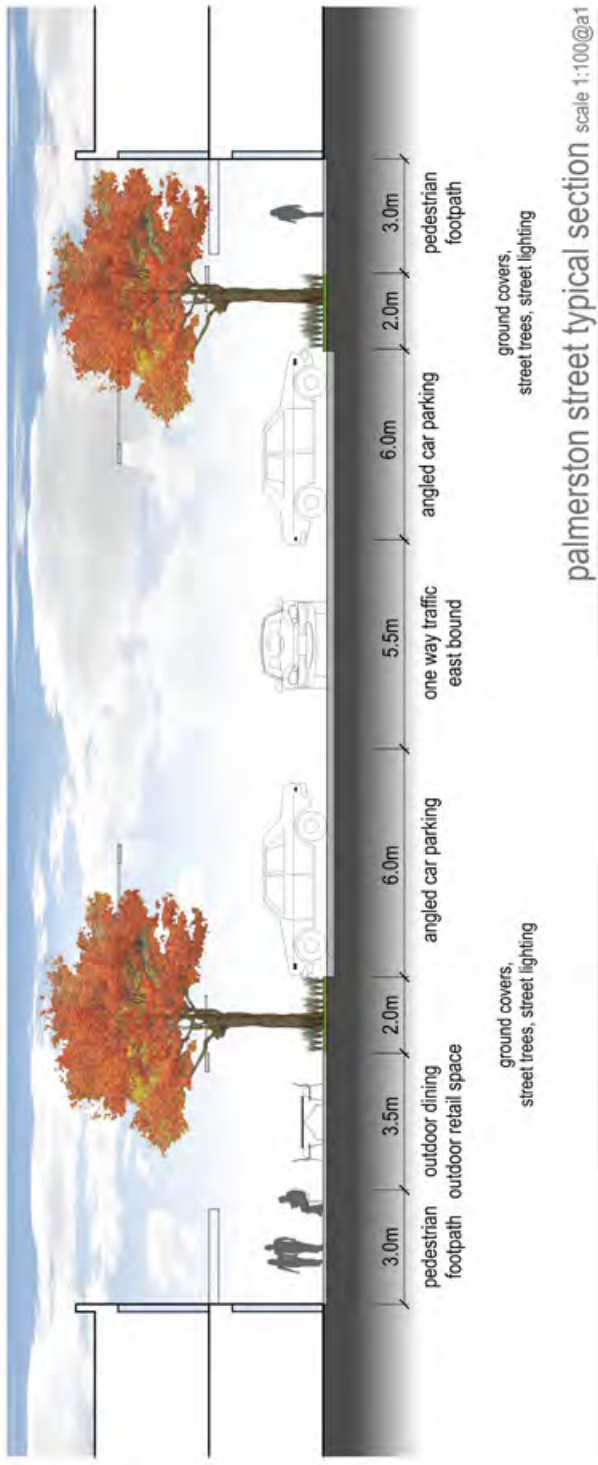
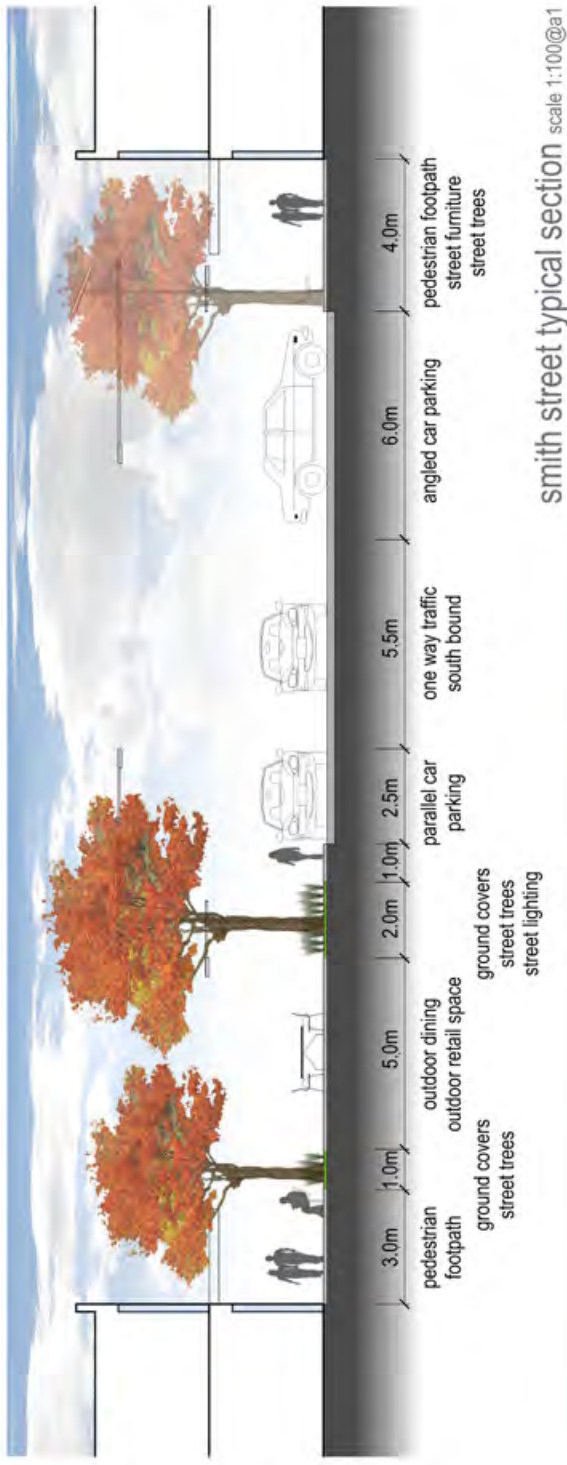
roundabout option detail plan

raise pavement of the roundabout to create a slower traffic environment for pedestrians
smith street / palmerston street intersection





smith st / palmerston st



- actions**
- 1 reduce vehicular traffic along palmerston and smith streets by reconfiguring them to offer one way traffic movement only and bus interchange
 - 2 retain angle parking
 - 3 provide a pedestrian and vehicular shared zone within the palmerston street elbow. this open space can be used for large community events by closing the street off to traffic
 - 4 incorporate a freestanding community kiosk café within the palmerston street shared zone
 - 5 retain the existing heritage listed elm tree as a dominant visual feature of palmerston street
 - 6 reconfigure car parking along palmerston street to provide a wide pedestrian promenade along the southern side which can be used for outdoor dining and retail
 - 8 provide pavement upgrades throughout palmerston and smith street
 - 9 incorporate new street tree planting to create strong vegetated avenues and establish the 'green spine' along smith street
 - 10 retain the existing angled car parking on the east side of smith street
 - 11 rearrange the car parking on the west side of smith street to a parallel arrangement
 - 12 establish a wide pedestrian boulevard, with space for outdoor dining and retail along the west side of smith street

3 smith street green spine

background

The Warragul Town Centre Urban Design Framework promotes the principle of a 'green link' linking recreation areas south of the town to Civic Park in the north. The top end of Smith Street forms an important component of this link.

vision

The Smith Street corridor seeks to establish a 'Green Spine' linking Civic Park in the north to the Burke Street Recreational Park on the southern side of the railway line, via the Warragul town centre. The 'green spine' is intended to introduce 'greenery', in the form of street tree planting and associated garden beds, and encourage a more environmentally sustainable approach to the use of Smith Street, by emphasising its value as a key pedestrian spine within the town centre. This notion of environmental sustainability is to be further emphasised through the introduction of Water Sensitive Urban Design to the Smith Street corridor, in the form of vegetated bio-retention basins to capture and treat urban stormwater runoff.

The Smith Street 'Green Spine' contributes to the achievement of the following Urban Design Framework Principles:

- **Principle 1** – Create a 'green link' through the town centre
- **Principle 4** – Prioritise pedestrian amenity
- **Principle 7** – Establish a clear road hierarchy
- **Principle 12** – 'Green' the town centre

actions

- Provide additional street trees within the Smith Street corridor to establish an attractive green avenue.
- Establish new planting beds in existing bare garden beds and median breaks, kerb outstands and traffic islands.
- Establish vegetated bio-retention basins within surplus paved areas adjacent to parking bays as a means of implementing Water Sensitive Urban Design principles, to capture and treat urban stormwater runoff.
- Create a central median along Smith Street between Albert Street and Palmerston Street and again between Victoria Street and Queen Street as a means of regulating vehicular traffic movement and providing a physical and visual refuge for pedestrians crossing the street.
- Provide formalised pedestrian zebra crossings at key mid-block locations between Albert Street and Victoria Street, as a means of facilitating safe and comfortable pedestrian access at locations separate from the key intersection roundabouts.
- Reconfigure the intersection of Smith Street and Barkly Street to provide for left and right turns in and out of both streets. This reconfiguration would need to occur in conjunction with the redevelopment works as envisaged for the Triangle Precinct.

probable cost

Smith Street Green Spine
(Between Albert Street and Palmerston Street)
Approximate Total Excluding GST: \$353,625.00

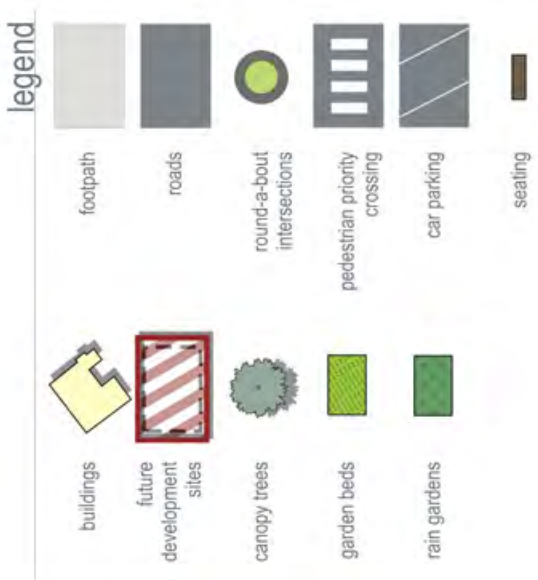
For a breakdown of costings please refer to Appendix 1 on page 43



smith street green spine

this project seeks to establish a green spine, linking civic park in the north to the burke street recreational park south of the rail line. this formal green corridor will comprise a strong street tree avenue, groundcover planting and rain gardens to collect and treat water runoff from the road.

the southern section of this project is covered in the smith and palmerston street master plan.



- actions
- 1

locate additional street trees throughout smith street to provide an attractive green avenue
- 2

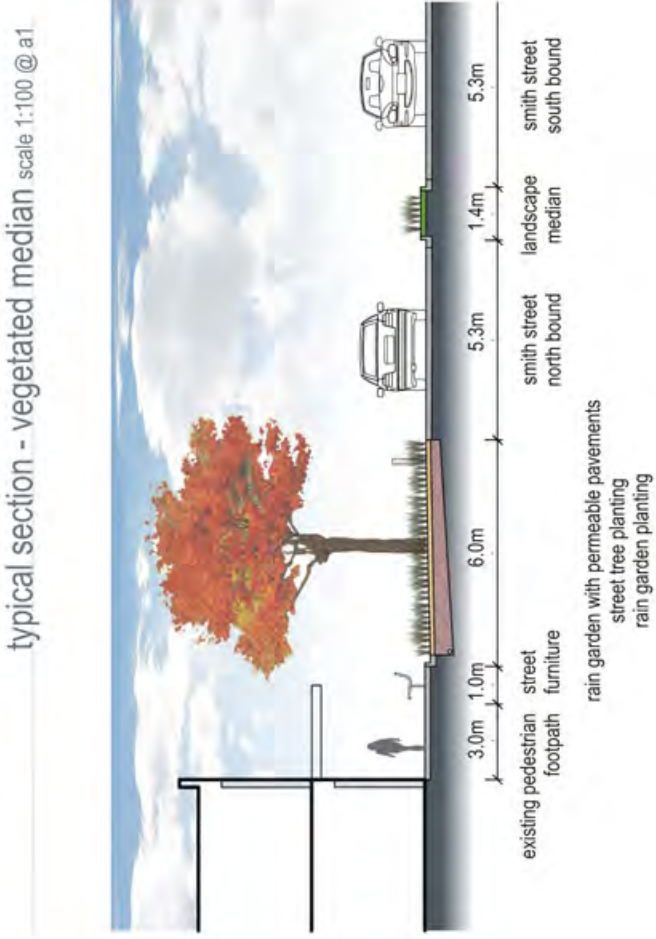
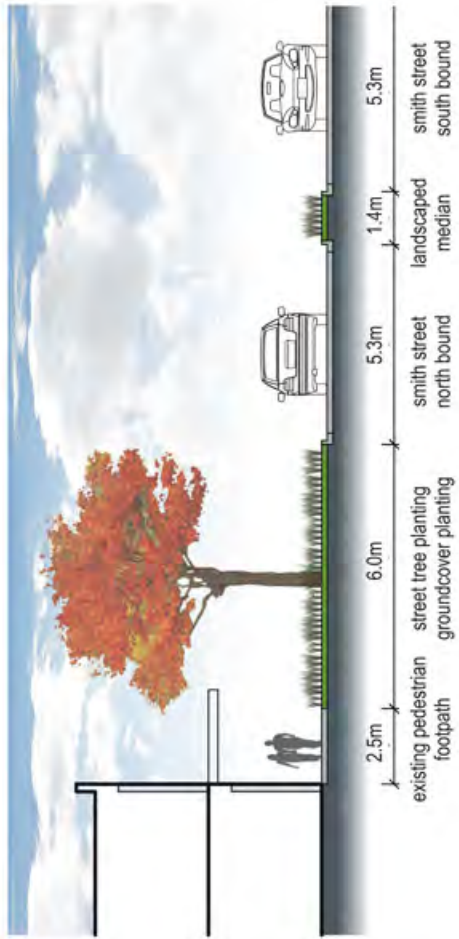
establish new groundcover planting within existing bare garden beds, lawn medians and traffic islands
- 3

create rain gardens within unused asphalt areas to capture and treat water runoff before discharge into the stormwater system
- 4

create a central median along smith street with landscaping and gravel paving to allow pedestrian movement across the road
- 5

formalise pedestrian priority zebra crossings
- 6

reconfigure smith/barkly street intersection in association with redevelopment of triangle precinct



typical section - rain garden scale 1:100 @ a1

4 trinca lane

background

Trinca Lane is currently an unsealed access track abutting the rear of properties which address Queen Street to the north. This configuration, along with the prevailing topography, which falls considerably from Queen Street to Trinca Lane, results in generally poor presentation of the Trinca Lane corridor as viewed from the railway corridor to the south. As one of the key gateways to Warragul, this precinct offers significant opportunities for improvement.

vision

The vision for Trinca Lane is focused on improving the experience for visitors arriving in Warragul by train. The vision comprises the formalisation and sealing of the Trinca Lane carriageway to facilitate improved access to properties along its length, as well as new landscaping and pedestrian access improvements to improve the physical appearance of the laneway, and particularly to improve its appearance as viewed from the rail corridor.

Trinca Lane will evolve with a high quality and engaging built form which recognises the visual prominence of this streetscape as viewed from the railway corridor. The western gateway at the intersection of Trinca Lane and Queen street will anchor this precinct, whilst a transitional form of two to three storeys along the remainder of Trinca Lane with windows, balconies and frontages encouraging the regeneration of Trinca Lane with a sense of place.

The Trinca Lane plan contributes to the achievement of the following Urban Design Framework Principles:

- **Principle 4** – prioritise pedestrian amenity
- **Principle 7** – establish a clear road hierarchy
- **Principle 8** – identify appropriate development opportunities

actions

- Establish Trinca Lane as a 3.5 metre wide sealed carriageway.
- Establish a 1 metre high visually-permeable safety fence along the top of the railway embankment.
- Undertake landscape improvements both within the Trinca Lane road reserve and along the railway embankment. Improvements should comprise native canopy trees and associated low understorey planting to create a landscaped corridor within which existing and future buildings will be visually recessive once landscaping has established.
- Buildings adjacent Trinca Lane must meet the following guidelines:
 - Provide a landscaped setback of 3m to Trinca Lane.
 - Buildings incorporate a building façade height of no more than three storeys.
 - Buildings incorporate windows, balconies and entrances to activate the Trinca Street streetscape.
 - Buildings are a high architectural and urban design quality, and are well articulated with setbacks, variations in materials, and a mix of projecting and recessed elements that have a regular vertical rhythm.
 - Design detailing is well resolved to avoid ‘back of house’ presentation to the railway line interface.



trinca lane

trinca lane is currently an unsealed road abutting the rear of properties along queen street, presenting poorly to visitors arriving to warragul via the rail line.

the trinca lane concept seeks to achieve two things, improve vehicular access along this road by converting it into an asphalt sealed surface and establishing landscaping, such as street tree planting to improve its appearance as an important entrance into the town as viewed from the railway line.



existing conditions



5 laneways

background

The network of laneways within the Warragul town centre play an important functional role in the provision of access, including for service vehicles to the rear of premises within the town centre. The laneways also provide incidental access for pedestrians and cyclists. Due to their physical condition the laneways are typically uninviting spaces for pedestrians and are perceived as being unsafe, particularly at night time.

vision

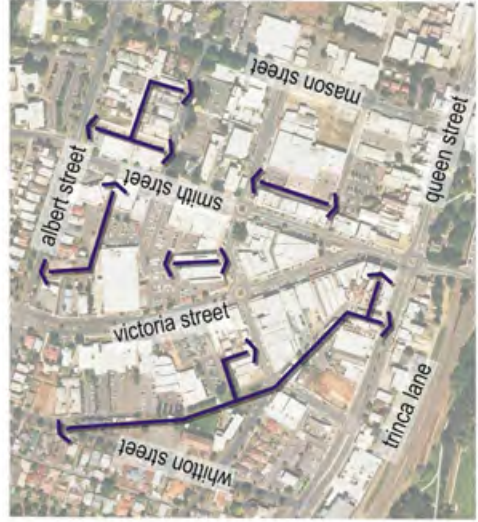
The vision for the Warragul town centre's laneway network is to explore opportunities to transform some of these public spaces into 'special places' within the context of Warragul's public spaces. There are a range of measures which can be implemented to achieve this outcome, ranging from relatively simple improvements to lighting and pavement surfaces, to more creative opportunities involving the incorporation of public art. Improvements to the laneways also need to be fundamentally concerned with activation of these spaces, through the establishment of a program of community events which can utilise laneway spaces, and hence encourage people to visit and occupy these spaces.

The Laneways plan contributes to the achievement of the following Urban Design Framework Principles:

- **Principle 2** – integrate new development
- **Principle 4** – prioritise pedestrian amenity
- **Principle 7** – establish a clear road hierarchy

actions

- Establish a Laneways committee to identify opportunities for the activation of existing laneways within the Warragul town centre.
- Implement required physical upgrade works to make the laneways safe for pedestrian and community events usage.
- Ensure access is retained for service and other vehicle access.



existing laneways

laneways

the laneways throughout the warragul town centre play an important role for cars and service vehicles, however they are uninviting spaces for pedestrians.

the aim of this project is to investigate how to improve pedestrian safety and accessibility within these lanes. the project will also explore the opportunities to make these special spaces within the town through activity and the incorporation of public art.



narrow
between club lane & smith street



shop frontages
club lane



shortcut
lane behind coles



car parking
club lane



dark
between club lane & smith st.



car priority
club lane



inside
between club lane & smith street



outside
club lane

current conditions



trees



colour



outdoor dining



art



art



activity



covered



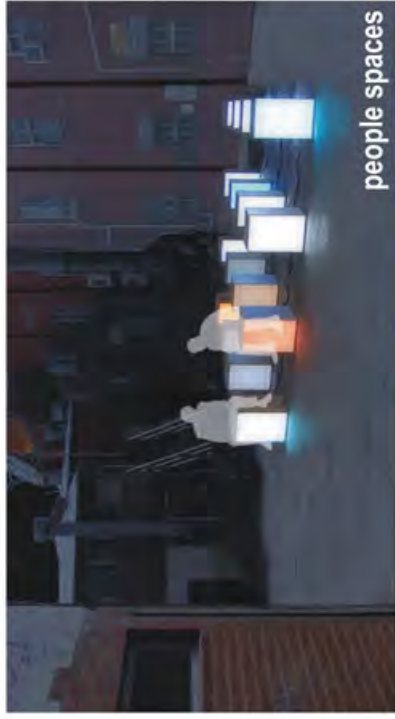
light



light



pattern



people spaces

opportunities

6 civic precinct

background

The 'Civic Precinct' consists of Civic Park, the West Gippsland Arts Centre and surrounding Council facilities. The Warragul Town Centre Urban Design Framework promotes the retention of the area as a key community and civic space.

vision

The vision for the Warragul Civic Precinct is to investigate opportunities for future development within this precinct aimed at reinforcing its important role as the primary civic, arts and cultural precinct of Warragul.

The vision for the precinct is preliminary only and is intended to provide a suite of guiding principles for future reference as opportunities arise to invest in improvements to the precinct.

The Civic Precinct Concept contributes to the achievement of the following Urban Design Framework Principles:

- **Principle 8** – identify appropriate development opportunities

guiding principles

- Retain existing canopy trees throughout the former swimming pool site.
- Establish screening vegetation along all residential interfaces.
- Provide a strong visual connection between the former swimming pool site and Civic Park.
- Encourage extension of public open space over old pool site of civic park to have a strong presentation to Albert Street.
- Investigate opportunities for a café, to support the activities occurring within the park.
- Explore opportunities to provide artwork within a new public open space on the former swimming pool site.
- Explore opportunities to promote the development of the existing council office site for uses that promote and reinforce its civic importance and revitalises the space as a key precinct within the context of the Warragul town centre.
- Ensure that any new development provides a strong presence and address to the important intersection of Smith and Albert Streets, recognising the civic prominence of this corner location.
- Retain the existing Warragul Arts Centre and ensure that new development forms respect and do not obstruct key views to this landmark building.
- Ensure that new development is accommodated with a generous entry forecourt space addressing Albert Street.
- Ensure new development is sufficiently set back from Albert Street to ensure a clear line of sight from the Smith/Albert Streets intersection to new parkland on the former swimming pool site.
- Ensure strong pedestrian connectivity throughout the precinct.
- Provide for clear delineation of pedestrian and vehicular entry points to the precinct, and encourage opportunities for the provision of shared access space.
- Ensure strong visual and physical connections between new development forms and adjacent parkland open space.



civic precinct concept



















the civic precinct project investigates the opportunity for future development within the precinct to reinforce its role as a valuable community, arts and cultural asset for warragul.

to achieve this, the master plan is providing initial guidance upon how the site could be developed by establishing a number of guiding principles



plan scale 1:500 @ a1

legend

- | | | | |
|---|---|---|---------------------------------------|
|  | existing trees |  | redevelopment opportunity |
|  | proposed trees |  | possible future built form |
|  | proposed screening trees |  | associated car parking |
|  | existing buildings to be retained |  | outdoor arts gallery / sculpture park |
|  | existing buildings possible redevelopment |  | possible future café |
|  | proposed public open space |  | pedestrian paths |
|  | existing public open space |  | existing roads |
|  | proposed pedestrian plaza |  | view lines |
|  | proposed pedestrian/vehicular shared way |  | visual connections |

- 1 retain the existing canopy vegetation throughout the pool site
- 2 establish screening vegetation along the residential boundaries
- 3 provide a strong visual connection between the old pool site and civic park
- 4 extend public open space over old pool site of civic park, providing a strong presentation to albert street
- 5 investigate the opportunity for a café to support the activities to occur within the park
- 6 explore the opportunity to provide artwork within the new public open space
- 7 explore opportunities to promote the development of the existing Council office site for uses that promote & reinforce its civic importance and revitalises the space as key precinct
- 8 ensure any proposed development forms respond to this prominent corner location at smith street
- 9 retain the existing arts centre and ensure any proposed development forms do not block views to this key visual landmark

- ## principles
- 10 proposed development to provide an entry forecourt space for pedestrians
 - 11 setback development to provide a clear sight line between the new public open space and the smith st / albert street intersection
 - 12 ensure strong pedestrian connectivity between smith street and the precinct / civic park
 - 13 provide a shared pedestrian / vehicular entrance into the precinct
 - 14 ensure any new development provides strong visual connections to the surrounding parkland

7 albert & gladstone streets

background

Albert Street and Gladstone Street form the northern and eastern periphery of the Warragul town centre core and are recognised as having a special character attributed to both the traditional building stock and treed streetscapes.

The Masterplan seeks to retain the attractive and unified residential scale buildings along Albert Street. Encourage these dwellings to be reused for commercial or consulting purposes where appropriate. The retention of these residential scale buildings is important in retaining the traditional character of the town with the added benefit of providing a transition in scale and intensity of use to the northern established residential areas.

vision

- Protect buildings with traditional character.
- Facilitate adaptive re-use of heritage buildings.
- Protect the landscape qualities of the streetscape.
- Retain the sense of spaciousness between buildings.

The Albert Street and Gladstone Street guidelines contribute to the achievement of the following Urban Design Framework Principles:

- **Principle 8** – identify appropriate development opportunities

guidelines

- Avoid demolition of buildings which contribute to the consistency of heritage building character
- Facilitate re-use of existing buildings for a mix of office, consulting, home office medical and other commercial or community uses that can be accommodated within existing dwellings and in accordance with the land use zoning.
- Limit modifications to the facades of buildings which would substantially alter the contribution to the streetscape
- New building additions should be setback the length of the first two rooms of the building (equivalent 8m depth of the dwelling frontage)
- Side setbacks of additions should maintain a minimum 1.5m setback from side boundaries and incorporate landscaping areas in the setback
- Building additions, in particular first floor additions, should be recessive
- Conceal car parking to the sides or rear of buildings.
- Encourage landscaping in the front and side building setbacks
- New additions to buildings should be contemporary in character, however they should seek to integrate with existing buildings by reflecting the proportions and scale, form, materials and setbacks of the existing building
- Roof forms of development should seek to maintain similar proportions and pitch as existing dwellings on/immediately adjacent the site.
- Materials and finishes should reflect the local palette and comprise predominantly weatherboard with selective use of render and brick. Reflective or metallic finishes are discouraged.

figure 9 future development principles



protect street trees



recessive building additions



protect heritage building stock

8 public area materials guidelines

Public area improvements within the Warragul Town Centre will utilise a coordinated suite of materials, furniture and plant species to ensure quality streetscapes with high levels of pedestrian amenity, visual character, safety and flexibility to accommodate a multitude of outdoor activities. The following guidelines provide direction with respect to the selection of appropriate materials and finishes, and should be used to inform future detailed design of specific public area improvement projects.

8.1 pavements

Pavement materials should be selected for their visual quality, robustness, slip resistance capabilities, availability and low requirement for ongoing maintenance.

8.1.1 Pedestrian pavements - general

Concrete pavements with an exposed aggregate finish are recommended for general area pedestrian paving. This material can be used in either in-situ applications or as unit pavers. Exposed aggregate concrete provides maximum flexibility for colour and finish selection, as well as providing excellent slip resistance.



8.1.2 Pedestrian pavements – permeable surfaces

For areas associated with proposed water sensitive urban design treatments, interlocking permeable pavers are recommended. These interlocking pavers are a coloured concrete unit with

recesses cast in to the paver units to allow for surface water to infiltrate below ground. They are available in a range of different colours and finishes.



8.2 Street furniture

A suite of coordinated street furniture elements is recommended to establish and reinforce a preferred streetscape character. Street furniture elements should be selected for their visual quality, robustness, comfort, availability and low requirement for ongoing maintenance.

8.2.1 Bench seats

Bench seats should provide seating opportunities for all members of the community. In this regard, they should be of a design which provides for their use by older persons and in particular persons of limited mobility, including wheelchair users. Bench seats with arm-rests are preferred for these reasons. In terms of materials, a combination of stainless or galvanised steel frames and recycled hardwood slats is recommended, for both longevity and comfort. Bench seats should be located in places where people are encouraged to gather, as well as at regular intervals along footpaths to provide opportunities for rest. Wherever possible they should be afforded some weather protection to ensure their usability at all times.



8.2.2 Rubbish bins

Rubbish bins should be provided in configurations which encourage users to recycle, by providing recycling bins alongside standard rubbish bins. Rubbish bins need to be selected on the basis of the ability for maintenance workers to access them regularly for rubbish removal, as well as being located in areas where people are encouraged to gather. In areas where outdoor dining is encouraged, a higher concentration of bins may be necessary to deal with anticipated demand. Rubbish bins should be powdercoated or stainless steel for longevity.



8.2.3 Bicycle rails

Bicycle rails should be provided to encourage travel to the Warragul Town Centre using non-motorised forms of transport. Bicycle rails are typically located close to activity nodes, whereby users can find convenient places to secure their bicycles in close proximity to their destination. Poorly-located bicycle rails are unlikely to be utilised. Bicycle rails should be located so that they do not impede the free movement of pedestrians, yet should be in highly visible locations where passive surveillance is available. Users typically do not favour 'out of sight' locations as they risk bicycles being stolen.



8.2.4 Drinking fountains

Drinking fountains should be located in all areas where people are encouraged to gather. They should be of a design which allows for use by people with limited mobility, and in particular they must be accessible for use by people in wheelchairs. Stainless steel materials are favoured for maintenance and longevity considerations. It should be noted that the ability to provide service connections (power, water and sewer) will influence the location of drinking fountains.



8.3 Plant species

The presence of plant species within public areas, both in the form of garden beds and street trees, is critical for the establishment of high quality, high amenity streets. Street trees provide a major contribution to the town's character, as well as providing amenity through shelter from sun and wind. Garden beds provide for landscape highlights within streets, as well as assisting in the demarcation of spaces within public areas. Rain gardens provide spaces for the application of Water Sensitive Urban Design principles, to improve the environmental performance of streetscapes and reduce reliance on stormwater infrastructure.

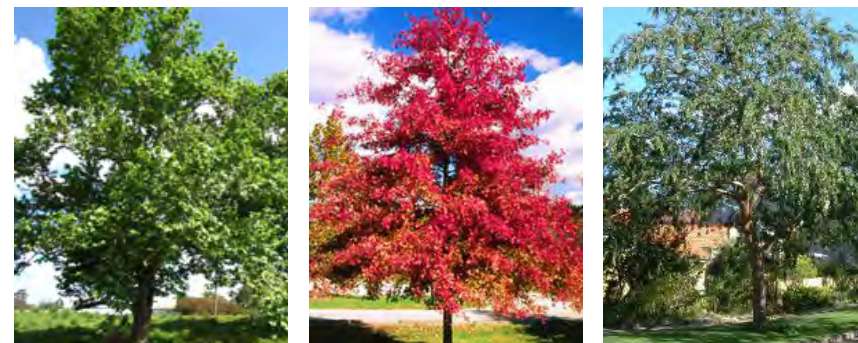
8.3.1 Street trees

Street trees should be deciduous species, to ensure consistency with the existing street trees within Warragul and to ensure adequate provision of sunlight in winter and shade in summer. Suggested species include:

Quercus palustris (Pin Oak)

Platanus orientalis (Oriental Plane)

Ulmus parvifolia (Chinese Elm)



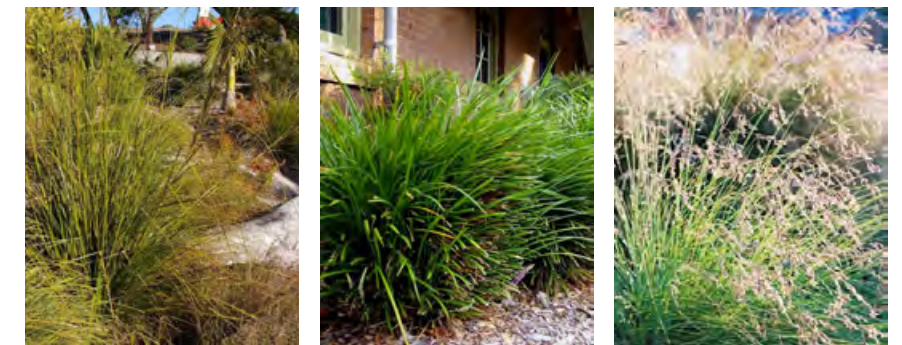
8.3.2 Garden beds

Garden beds should be provided to complement those already prominent in Warragul's town centre streets. These beds should be planted with low-growing, low-maintenance, ornamental shrub and groundcover species.



8.3.3 Rain gardens

Rain gardens should be provided where opportunities arise to capture and treat urban runoff prior to discharge to traditional underground stormwater infrastructure. These beds should be planted with indigenous species which are suited to the anticipated conditions, and hardy enough to thrive in these relatively hostile environments.



warragul town centre master plan

master plan principles

legend

retail consolidation	
mixed use development	
concealed car parking	
retain heritage buildings	
widened laneway	
active frontage	
animated frontage	
landscaped frontage	
sensitive interface treatments	
existing street network	
future primary connection	
future secondary connection	
pedestrian thoroughfare	
service access	
pedestrian crossing	



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warragul town centre master plan

floor plan - basement level

legend

- site 
- carparking 
- vehicular access 



floor area schedule - basement level

triangle precinct			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	0	0
car parking	0	18,260	18,260
residential	0	0	0
public space	0	0	0
william sq precinct			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	0	0
car parking	0	0	0
residential	0	0	0
public space	0	0	0
butter factory			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	0	0
car parking	0	7,230	7,230
residential	0	0	0
public space	0	0	0

*retail - DDS, supermarket, mini majors, specialty retail
*non retail - community facilities, mixed use

























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warragul town centre master plan

floor plan - ground level

legend

- site 
- existing heritage site 
- existing council car parking 
- existing buildings 
- mixed use 
- commercial- DDS
(discount department store) 
- commercial- supermarket 
- commercial- mini majors 
- commercial- specialty retail 
- community facility 
- residential 
- public space 
- share zone 
- pedestrian & vehicle 
- car parking 
- vehicular access 
- service access 
- service loading bay 
- main street 
- main street (future extension) 
- lane way (future extension) 
- pedestrian access 

floor area schedule - ground level

triangle precinct			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	6,820	10,940	4,120
non retail*	0	1,810	1,810
car parking	10,860	0	0
residential	0	0	0
public space	0	2,520	2,520

william sq precinct			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	5,890	12,740	6,850
non retail*	0	0	0
car parking	4,850	2,280	-2,570
residential	0	0	0
public space	0	0	0

butter factory			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	7,190	7,190
non retail*	0	11,330	11,330
car parking	0	4,500	4,500
residential	0	5,485	5,485
public space	0	0	0

*retail - DDS, supermarket, mini majors, specialty retail
*non retail - community facilities, mixed use



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warragul town centre master plan

floor plan - first level

legend

- site
- existing heritage site
- mixed use
- commercial- DDS (discount department store)
- commercial- supermarket
- commercial- mini majors
- community facility
- residential
- car parking
- vehicular access

floor area schedule - first level

triangle precinct			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	4,550	4,550
car parking	0	0	0
residential	0	0	0
public space	0	0	0

william sq precinct			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	0	0
car parking	1,180	2,280	1,100
residential	0	0	0
public space	0	0	0

butter factory			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	11,330	11,330
car parking	0	4,500	4,500
residential	0	5,485	5,485
public space	0	0	0

*retail - DDS, supermarket, mini majors, specialty retail
*non retail - community facilities, mixed use



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warragul town centre master plan

floor plan - second level

legend

- site
- residential
- car parking
- vehicular access

floor area schedule - second level

triangle precinct			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	0	0
car parking	0	0	0
residential	0	0	0
public space	0	0	0
william sq precinct			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	0	0
car parking	0	14,850	14,850
residential	0	0	0
public space	0	0	0
butter factory			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	0	0
car parking	0	4,500	4,500
residential	0	5,485	5,485
public space	0	0	0

*retail - DDS, supermarket, mini majors, specialty retail
*non retail - community facilities, mixed use



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warragul town centre master plan

floor plan - third level

legend

site 

car parking 

vehicular access 

floor area schedule - third level

triangle precinct			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	0	0
car parking	0	0	0
residential	0	0	0
public space	0	0	0
william sq precinct			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	0	0
car parking	0	0	0
residential	0	0	0
public space	0	0	0
butter factory			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	0	0
car parking	0	4,500	4,500
residential	0	0	0
public space	0	0	0

*retail - DDS, supermarket, mini majors, specialty retail
*non retail - community facilities, mixed use



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warragul town centre master plan

floor plan - fourth level

legend

site

car parking

vehicular access

floor area schedule - fourth level

triangle precinct			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	0	0
car parking	0	0	0
residential	0	0	0
public space	0	0	0

william sq precinct			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	0	0
car parking	0	0	0
residential	0	0	0
public space	0	0	0

butter factory			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	0	0
non retail*	0	0	0
car parking	0	4,500	4,500
residential	0	0	0
public space	0	0	0

*retail - DDS, supermarket, mini majors, specialty retail

*non retail - community facilities, mixed use

floor area schedule - all levels and precincts

triangle precinct (all levels)			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	6,820	10,940	4,120
non retail*	0	6,360	6,360
car parking	10,860	18,260	7,400
residential	0	0	0
public space	0	2,520	2,520

william sq precinct (all levels)			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	5,890	12,740	6,850
non retail*	0	0	0
car parking	6,030	19,410	13,380
residential	0	0	0
public space	0	0	0

butter factory (all levels)			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	0	7,190	7,190
non retail*	0	22,660	22,660
car parking	0	29,730	29,730
residential	0	16,455	16,455
public space	0	0	0

all levels and precincts*			
landuse	existing floor area (sqm)	proposed floor area (sqm)	net increase (sqm)
retail*	12,710	30,870	18,160
non retail*	0	30,790	30,790
car parking	16,890	67,400	50,510
residential	0	16,455	16,455
public space	0	2,520	2,520

*retail - DDS, supermarket, mini majors, specialty retail

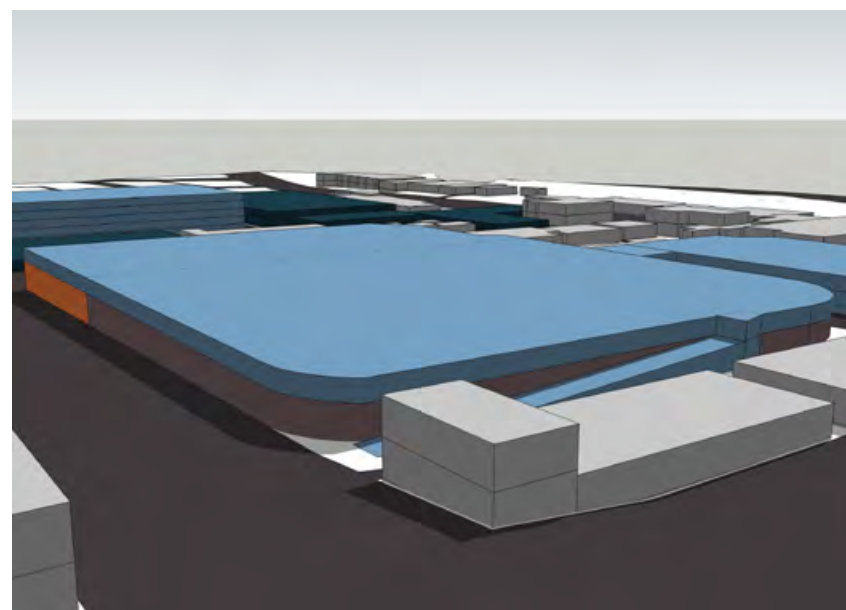
*non retail - community facilities, mixed use

*all levels and precincts - all levels in the triangle precinct, williams sq precinct and butter factory combined

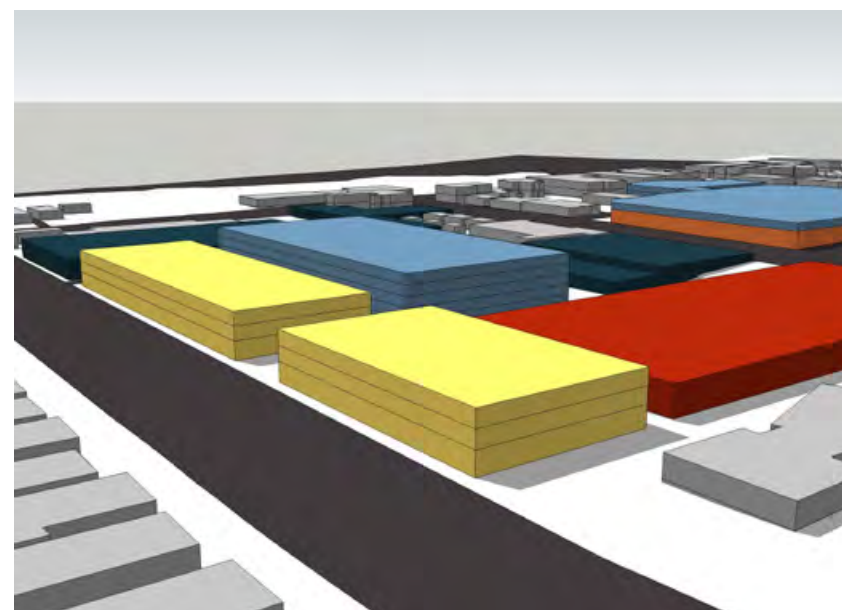


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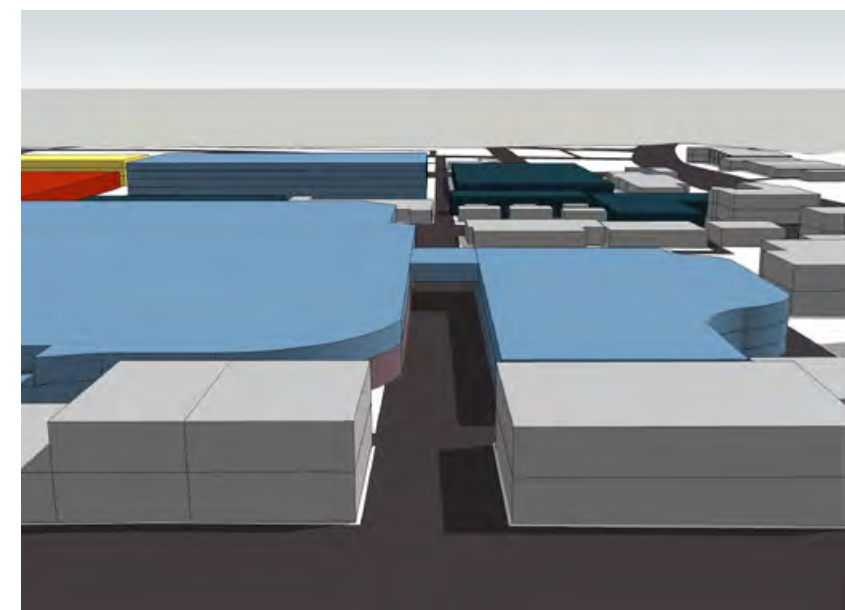
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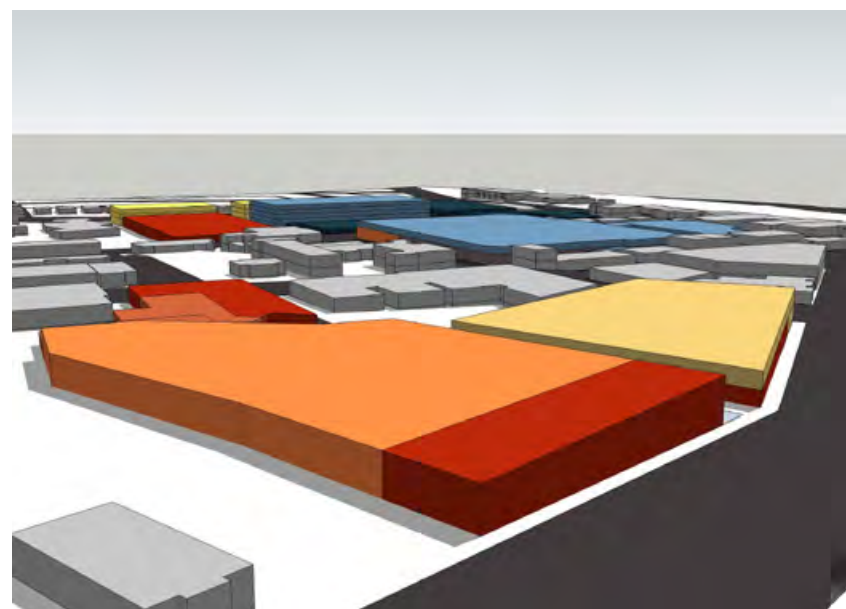
palmerston / smith street view to williams square precinct



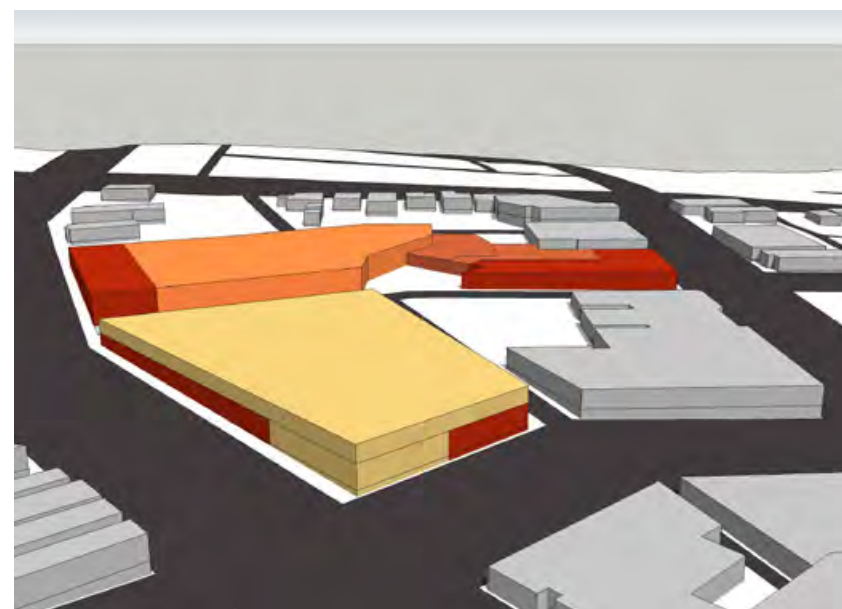
gladstone street view south west to butter factory precinct



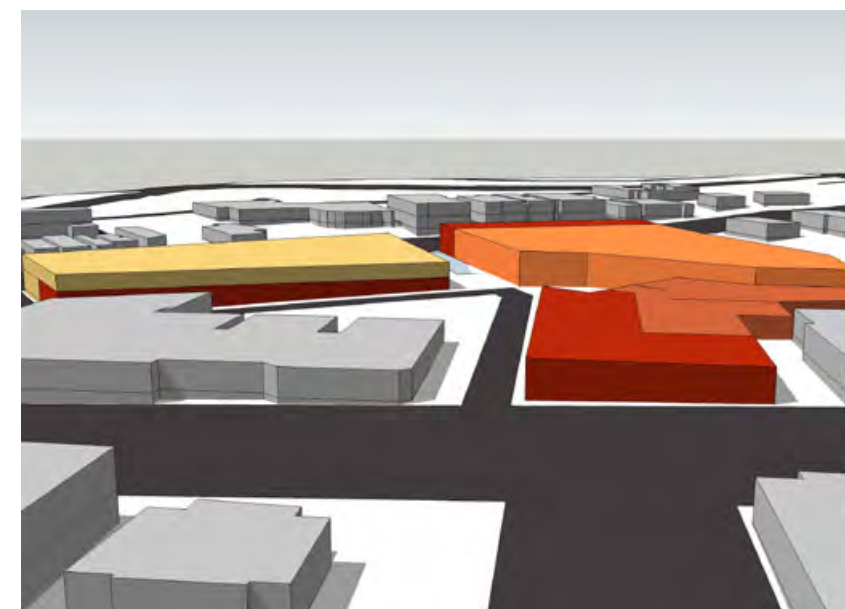
smith / williams street view east through williams square precinct



victoria street view south east towards triangle precinct



victoria / palmerston street view north towards triangle precinct



smith / barkly street view west towards triangle precinct

9.1 triangle precinct

The Triangle Precinct incorporates a number of land holdings distinguished by the occupancy of Woolworths, Woolworths Petrol, Target Country and extensive at grade car parking in between. There are existing arrangements in relation to car parking restrictions which are recorded on the titles for the land holdings.

There is a notable slope rising from south to north which forms a key site constraint. The existing open lot car parking areas create extensive gaps in street edge development and currently detract from the pedestrian amenity at the site edges.

The existing service access adjoining the south-eastern boundaries of the development should be maintained for the adjoining Smith Street and Palmerston Street properties unless alternative access arrangements can be found.

vision

To redevelop a key site in the western part of the retail core to achieve the principles of the UDF. This precinct will establish an integrated retail and community hub anchoring activity at the western end of the Palmerston Street axis. The Triangle Precinct will reinvigorate the image of the retail core with an inspiring, articulated two to three storey form which steps up the site slope. It will incorporate a new multi-purpose community facility which activates the intersection of Victoria and Palmerston Streets and integrates with associated improvements to Palmerston Street (refer Chapter 2 Smith Street & Palmerston Street).

guidelines

land use

- Establish a mix of land uses which include the following:
 - A multi-purpose community centre
 - Large format retail such as a supermarket
 - A mix of mini-major retail
 - Specialty retail

parking and access

- New driveways and accessways should be located to maximise separation from existing and planned pedestrian crossings, with the primary access via Victoria Street.
- Conceal car parking in a basement and ensure the location of vehicle crossovers minimises impact to key pedestrian routes such as Palmerston Street. New driveways and accessways should be located to maximise separation from existing and planned pedestrian crossings, with a primary access via Victoria street.
- Maintain service laneway access to the rear of 41-47 Smith Street and 15-17 Palmerston Street unless alternative access arrangements can be found.
- Loading and storage areas should be concealed from public view to the rear of buildings or contained within the building wherever possible.
- Pedestrian spaces must be suitably designed for DDA compliance and where abutting service laneways, they should be established as a safely designed shared pedestrian/vehicle space.

built form

- Locate small scale active retail uses at ground level with doors and windows at the street level.
- Active retail should address the street frontages and public thoroughfares with clear glazing to maximise passive surveillance.
- Building entries, windows and balconies should be arranged to provide passive surveillance over public areas and adjoining streets.
- Establish a 'street wall' with a zero lot setback and a height of up to 12m (equivalent two to three storeys).
- Development above the street wall should be setback a minimum of 5m and should have a recessive presentation to the streetscape.
- Overall building height should not exceed 16m above natural ground level.
- Buildings should be articulated with regularly spaced vertical details that reflect the traditional fine grain frontage combined with a balance of horizontal detailing. Refer to figure 18.

- Buildings should include defined roof forms that 'cap' the overall presentation of buildings and contribute to a high quality presentation in the streetscape and skyline context of Warragul town centre.
- Plant and service equipment should be concealed from public views and incorporated into the built form.
- Avoid extensive and highly visible blank walls, and where these cannot be avoided ensure they are suitably articulated through materials, textures and patterns if visible in the short-medium term.

figure 19 indicative development vertical and horizontal detail

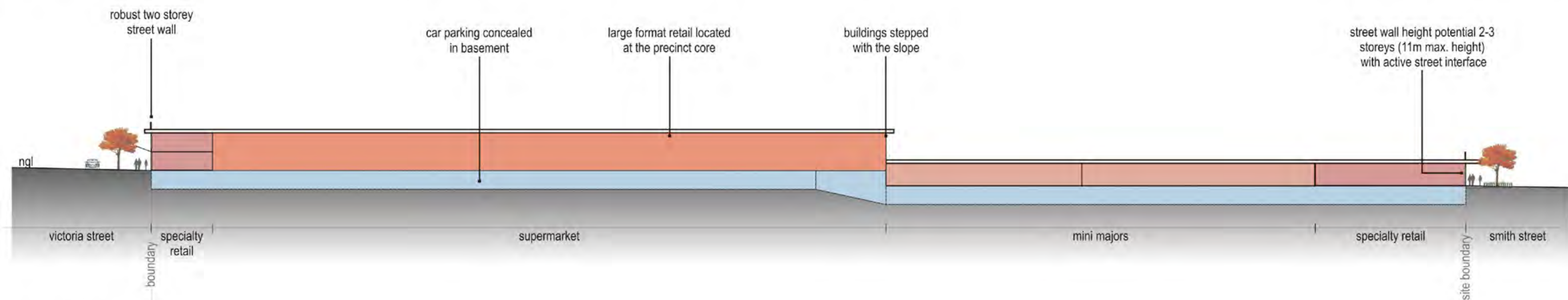




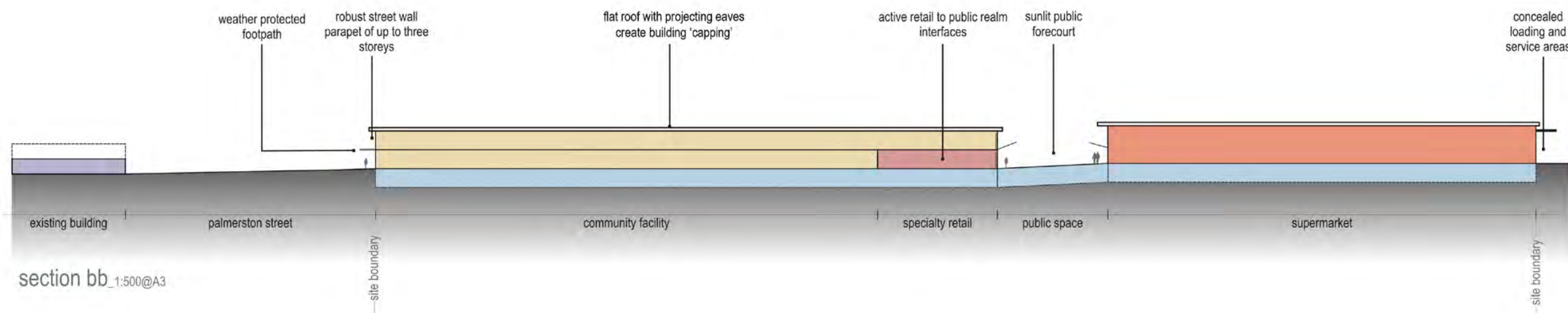
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warragul town centre master plan triangle precinct section



section aa_1:500@A3



section bb_1:500@A3



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9.2 williams square precinct

The Williams Square site comprise a mix of public and private land holdings comprising the following:

- Williams Street car park
- Centrepont shopping centre / Coles Supermarket site
- Vacant site fronting Mason Street
- A number of privately owned shops fronting Palmerston Street

Williams Street functions as a through-street, however the western extent permits one way traffic travelling east into the precinct from Smith Street. The development of the Williams Square precinct should prioritise other access points via Mason Street and Palmerston Street. The importance of regularly spaced intersecting streets is a key element of walkability, therefore development of this precinct should ensure the retention and enhancement of a safe, high amenity pedestrian movement network.

There is scope to comprehensively develop the entire precinct with large format retail uses, including a supermarket and discount department store. The design should prioritise building entries and active edges to Palmerston Street which forms part of the key pedestrian axis linking the three major development sites. Mason Street and Williams Street should also be suitably activated to avoid extensive blank walls. There is demand for commercial office space in the town centre, which can be accommodated within this precinct and contribute to diversity of uses and activity, passive surveillance and activation of buildings above street level.

The development should retain well located and accessible car parking. This may be achieved on the site either in a basement level or in a mix of decked and rooftop parking arrangements. Such facilities should be an integrated part of any design and should make a positive contribution to the presentation of the built form and hence should be setback from main frontages.

vision

This precinct will be redeveloped to accommodate a mix of retail uses which regenerate and consolidate the retail core and key pedestrian axis of Palmerston Street. It will realise the opportunity to provide centrally located large format retail uses supported by a range of other primary retail and commercial uses. Development will contribute to a positive public image and identity for Warragul and reflect contemporary activity centre design principles.

guidelines

land use

- A mix of retail uses including specialty retail, large format retail, discount department store and other commercial and entertainment uses are accommodated on the site.
- Land uses should be configured to provide active street frontages and enhance passive surveillance to the surrounding public areas.

parking and access

- Retain the Williams Square Council car park and redevelop with a maximum three storey deck car park subject to the following:
 - A maximum parapet wall height of 9m above natural ground level;
 - Setback a minimum of 6m from the south, east and west boundaries and minimise unreasonable loss of daylight to the frontage of interfacing private properties interfacing with the car park;
 - Any roof feature or weather protection over the third storey is lightweight in design and is well articulated;
 - The overall design makes a positive contribution to the pedestrian environment; and
 - Connect decked car parking areas on the Council and privately owned sites, as appropriate, to provide legibility and improved permeability of the precinct car parking.
- Maintain Williams Street as a public vehicle thoroughfare with pedestrian link along its southern edge.
- A permeable access network to be established throughout this precinct which gives priority to the safety and amenity of pedestrians and cyclists.
- Bicycle access and parking should be provided in conveniently accessible locations that have good natural surveillance and in proximity to key building entries.
- Provide for laneway widening (to a minimum width of 6m) along the western boundary of the Centrepont shopping centre site to accommodate service vehicles for the precinct.
- Locate and design car park access to maintain safety and amenity along key pedestrian thoroughfares such as Palmerston Street and Mason Street.

built form

- Above ground connections between public and private car parking over William Street should maintain adequate clearance for truck and service vehicle thoroughfare, and should be appropriately designed to create visual distinction to William Street.
- Conceal large retail forms (e.g. supermarket, mini major stores and discount department stores) behind active retail frontages which incorporate regularly spaced and well placed entries including highly glazed frontages at street level.
- Buildings should have a zero lot setback from Palmerston Street and Mason Street to create a consistent built form within the retail core of the activity centre.
- Establish a parapet street wall of up to 11m (and a maximum of three storeys).
- Development above the parapet street wall should be setback a minimum of 3m to have a recessive presentation
- Buildings demonstrate a high quality architectural and urban design response incorporating a balance of horizontal and vertical detailing, visually interesting materials and finishes palette, active street interfaces and a human scale form.
- Blank walls should be avoided to Palmerston Street and Mason Street and achieve a minimum 80% active frontage to each street, whilst buildings interfacing Williams Street should include at least 20% active frontages comprising logically placed building entries and glazed retail frontage which integrate with the surrounding development.
- Weather protection measures should be provided to the Palmerston Street, Mason Street and Williams Street (where appropriate and practical) building interfaces.
- Conceal car parking within the site boundaries and ensure such facilities have a visually recessive presentation from Palmerston Street, Smith Street and Mason Street.
- Achieve a high degree of design resolution and detail to decked above ground car parking to be articulated through the form, and construction materials and finishes that are visually interesting and integrate with the surrounding development.

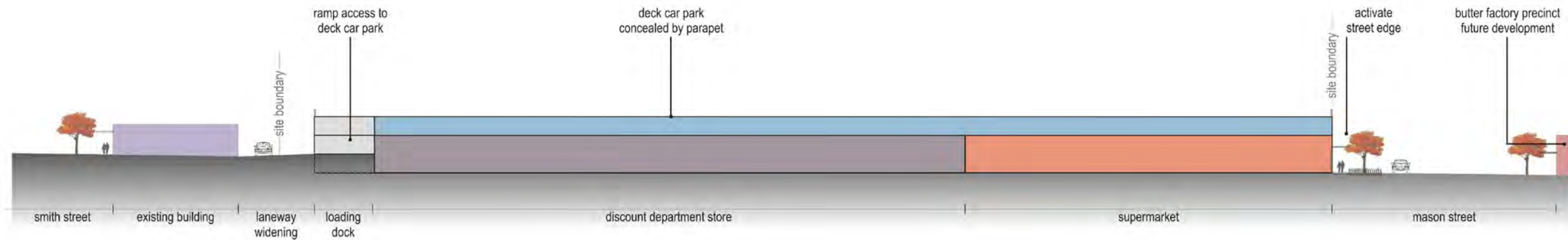


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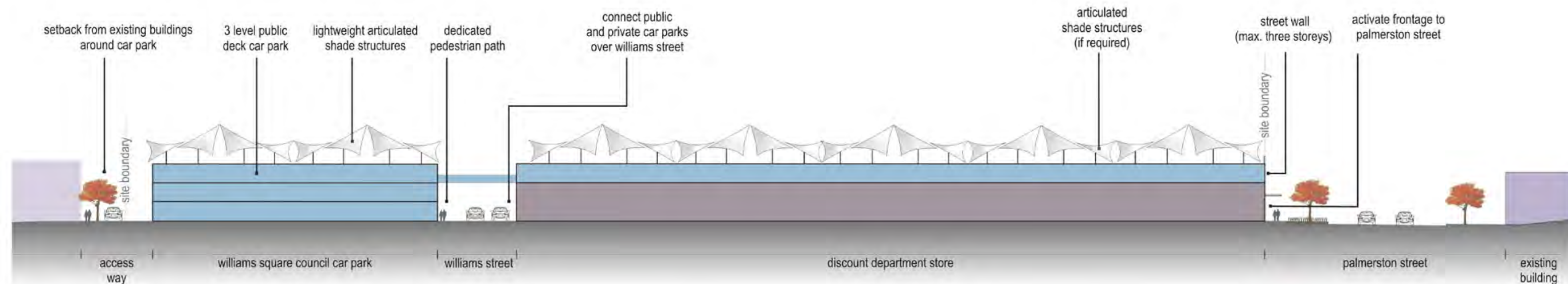


warragul town centre master plan

williams square precinct sections



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9.3 butter factory precinct

The Butter Factory Precinct comprises two key private land holdings, the larger comprising the butter factory and the smaller being the Commercial Hotel. The precinct is the designated location for expansion of the retail core of the Warragul town centre and is within the Business 1 Zone.

The former butter factory site has a planning permit for development of a retail precinct in a traditional enclosed shopping centre form. The development of the Butter Factory Precinct seeks to build upon the existing street hierarchy with greater site permeability including primary streets, secondary streets and laneways. The site has the capacity to accommodate a mix of retail, commercial office and residential opportunities.

vision

The Butter Factory Precinct will allow for orderly future expansion of the town centre primary retail core along the Palmerston Street axis to the east Mason Street. A mix of land uses will be configured around a street based network of vehicle accessways and pedestrian places. The development of this precinct will ensure that the site is progressively developed over time in accordance with retail and commercial land use demands and reflect best practice town centre design principles.

guidelines

land use

- A mix of land uses including primary retail, large format retail, offices, commercial, entertainment and residential uses will be accommodated on the site
- Primary active retail frontages should be focused along the Mason Street frontage and the Palmerston Street extension
- Large format retail uses should be located well within the site or incorporate activate frontages to key pedestrian thoroughfares
- Residential land use to be focussed at the Gladstone Street frontage (northern portion of the precinct) which may include a mix of townhouse, apartments or serviced accommodation
- Adaptive re-use of heritage buildings in the south of the precinct should provide for their retention and adaptation for a mix of uses which may include offices, galleries, cafes/ restaurant or retail

parking and access

- Provide a legible and well connected network of streets with safe pedestrian and bicycle access within the site which integrate with the broader activity centre network.
- Extend Palmerston Street as a wide tree lined street environment between Mason Street and Gladstone Street.
- Provide a series of north-south connections according to a street based design (vehicle, pedestrian, cycling) and landscaping typical of the rest of the activity centre.
- A shared pedestrian and vehicle access connecting to Williams Street will extend through the development to Gladstone Street.
- Vehicular access to decked or basement car parking to be provided at appropriately located access points from Mason and Gladstone Streets.
- At grade or decked car parking to be located centrally within the precinct, away from street frontages.
- Interim at grade car parks may be incorporated into the staging of development, however in the medium- long term car parking should be predominantly basement or decked car parks.
- Servicing and loading areas to be concealed within building envelopes to avoid conflict with pedestrian, bicycle or car parking areas.

buildings

- Retain the existing heritage brick buildings in the south-east corner of the precinct and ensure new development integrates in accordance with Council's heritage policies
- Retain the Commercial Hotel and sympathetically design new buildings to the rear/side which accord with the scale and proportions of the existing building.
- Buildings will provide design articulation and activation to the existing street frontages to Mason Street, Queen Street and Gladstone Street
- Commercial development to Mason Street must have a zero boundary setback with provision of weather protection measures over the public footpath.
- Commercial development facing Gladstone Street should be setback a minimum of 3m for soft landscaping.

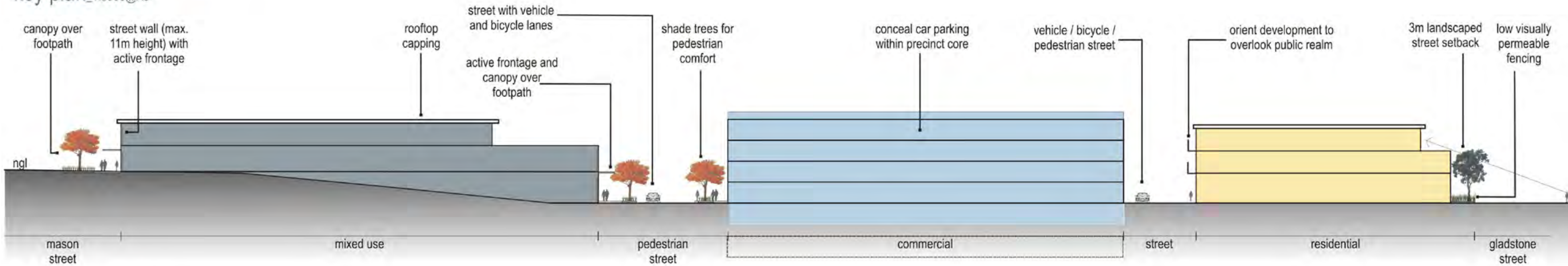
- Commercial development should be built to the street edge with a parapet street wall up to 11m (maximum three storeys) providing active frontages to street frontages, pedestrian shared zone areas and public thoroughfares.
- Development will be oriented to provide active frontages, building entries and articulated building interfaces to internal streets and on-site car parking area to maximise passive surveillance and security.
- At-grade or decked car parking areas will be designed with regularly spaced landscaping (at-grade online) and a high degree of architectural resolution to decked car parks with lightweight and articulated shade structures where applicable.
- Buildings will have a maximum parapet street wall height of 11m.
- Buildings will have a maximum height of three storeys (and 14m) measured at street level to the nearest street / accessway frontage from which the development will be visible.
- Buildings should be sited and designed to follow the site slope and avoid excessively elevated buildings which result in exposed blank walls.
- Residential buildings fronting Gladstone Street will:
 - Have a maximum height of 3 storeys (and 11m) to the street
 - Ground and first floor building setbacks a minimum of 3m from Gladstone Street for soft landscaping
 - Second floor building setback which achieves a recessive presentation when viewed from the opposite side of Gladstone Street
 - Provide for passive surveillance of the public realm with placement of building entries and windows at street level, balconies and windows above street level and visually permeable fencing treatments
 - Building articulation reflecting the rhythm and grain of the residential development along Gladstone Street
 - Have a contemporary design which reflects the design cues of the area such as building proportions and materials and finishes
 - Passive solar design principles to be integrated in the siting and design of dwellings, and further supported by energy and water efficient design



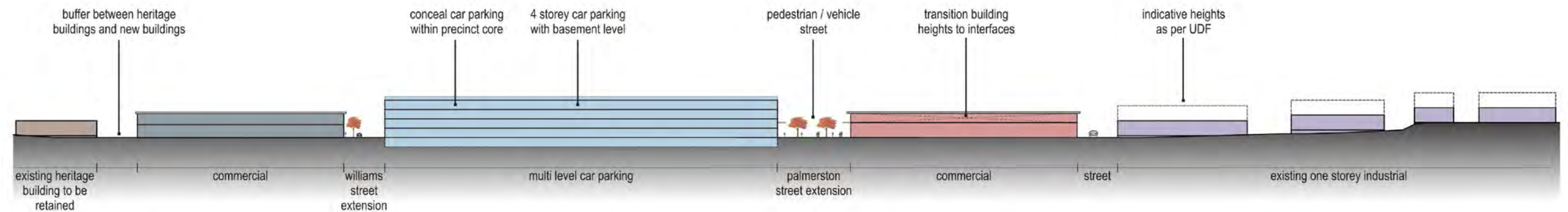
warragul town centre master plan

butter factory precinct sections

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9.4 gateway sites

Two gateways are identified as follows:

Southern Gateway at the intersection of Howitt Street and Alfred Street within the proposed bulky goods precinct. Howitt Street is a key conduit for vehicles entering the town from the south. Alfred Street at present is also an ‘on-ramp’ to the freeway to the east, in the future the anticipated third railway crossing underpass will increase the role and profile of Alfred Street.

Western Gateway which incorporates the land at the intersection of Queen Street and Trinca Lane identified in the UDF for future commercial development. The precinct is highly exposed for traffic entering the town from the western approach along Queen Street. The land is prominently positioned from this perspective, whilst also serving an important visual gateway to the railway line and Trinca lane. The land has a notable fall from Queen Street to Trinca Lane which offers the opportunity for semi-basement car parking to be provided on the site.

vision

To increase the overall attractiveness of primary entry roads and via the railway routes. Gateway development will revitalise the image and identity of the town centre. Development at the gateways should convey a positive image and identity for the Warragul town centre, demarcating these sites with notable building forms which are of a high architectural and urban design quality, making a positive contribution to the public realm.

southern gateway

- New development will include bulky goods retail uses and showrooms which maximise their prominence to Howitt Street and Alfred Street.
- Development should provide a robust presentation to the intersection of Howitt Street and Alfred Street through provision of logically placed building entries, highly glazed walls with good visual connection between public and private spaces, and articulated parapet street wall presentation.
- Development to be setback 5m from Howitt Street and Alfred Street.
- Development to be setback a minimum of 3m from other boundaries.
- A 3m soft landscape buffer is to be established around the site perimeter.
- On-site service and loading should occur to the eastern or southern interfaces.
- The heritage trees along Permewan Lane will be protected by locating vehicle crossovers via other street frontages with a preference for vehicle and service access via the southern laneway.
- Minimise vehicle access and visible car parking to the Howitt Street and Alfred Street frontages to enable the building and landscape features to be prominent.
- The building should be suitably capped with a roof form or feature parapet street wall with all plant and rooftop equipment concealed from the surrounding street views.

The general form anticipated for the Southern Gateway is illustrated in figures 23 and 24.

Based on the above guidelines, the following extent of development can be accommodated within the southern gateway site:

level	landuse	proposed floor area (sqm)
Basement (optional)	carparking / access	2,845
ground	bulky goods retail	2,575
	access, loading car parking	1370
	landscaping	2490
level 1	bulky goods retail	2,575
		Net leasable floor space total: 5,150 sqm
		Car parking and accessway total: 4215 sqm

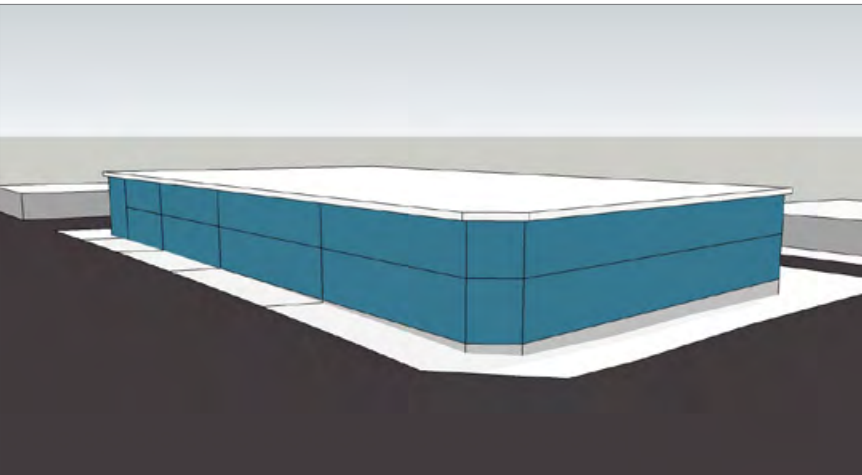
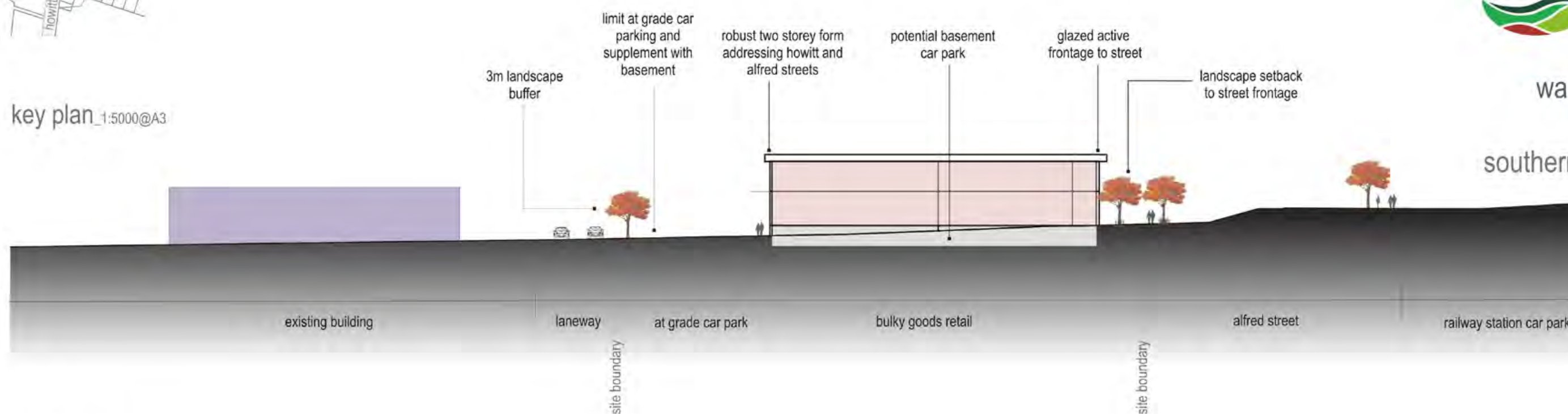


Figure 23 southern gateway view from the corner of howitt and alfred streets

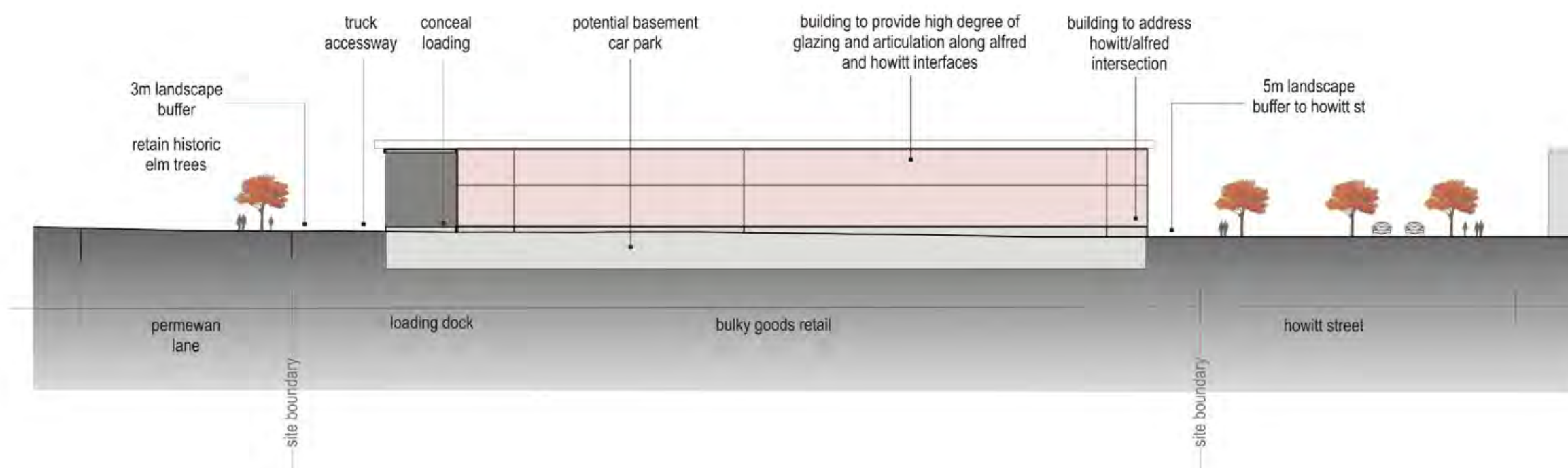


warragul town centre
master plan
southern gateway section

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western gateway

- Incorporate a mix of uses with active street frontages incorporating clear glazed windows and well located building entries.
- Provide a minimum 3m landscape setback from Trinca Lane.
- Provide a minimum 5m landscape setback from the eastern boundary for planting of canopy trees.
- Establish a parapet street wall of up to three storeys (11m) to Queen Street which transitions to two storeys (8m) at the eastern part of the site.
- A maximum building height of four storeys above the Queen Street footpath level which steps down to three storeys at the eastern part of the site.
- The western ‘wedge’ of the site will contain a distinctive corner feature treatment which wraps from the Queen Street frontage to Trinca Lane.
- Building entries should be logically placed and designed towards the Queen Street frontage with secondary access to Trinca Lane.
- Buildings should be sited and designed to provide passive surveillance of the adjoining public areas with a mix of building entries, windows and balconies.
- Buildings should be of a high architectural and urban design quality which are well articulated, responsive to the context and of an appropriate human scale.
- Buildings should be broken up across the site frontages with a mix of building breaks, generous building recesses and a regular vertical rhythm and grain established through design details.
- Development must ensure a high quality presentation of equal resolution is achieved to both the Queen Street and Trinca Lane frontages.
- Use the fall of the land to provide undercroft / semi- basement car parking which is concealed from views along Queen Street.
- Visitor or customer car parking may be accommodated within a setback from the eastern boundary provided a landscape buffer of 5m is provided.

- Vehicle access can be achieved via a maximum of one centralised vehicle crossover to Queen Street and/or vehicle access via Trinca Lane which is sited and designed to ensure that a row of shadow canopy trees can be established within/ adjacent to the site along this frontage.

Based on the above guidelines, the following extent of development can be accommodated within the southern gateway site:

level	landuse	proposed floor area (sqm)
basement	access and service loading	370
	carparking	5,010
ground	mixed use	5,010
	access and service loading	370
	landscaping	970
level 1	mixed use	5,010
level 2	mixed use	4,205
level 3	mixed use	1,385
		Net leasable floor space total: 15,610 sqm
		Car parking and accessway total: 5,750 sqm

The general form anticipated for the Western Gateway is illustrated in figures 25, 26 and 27.

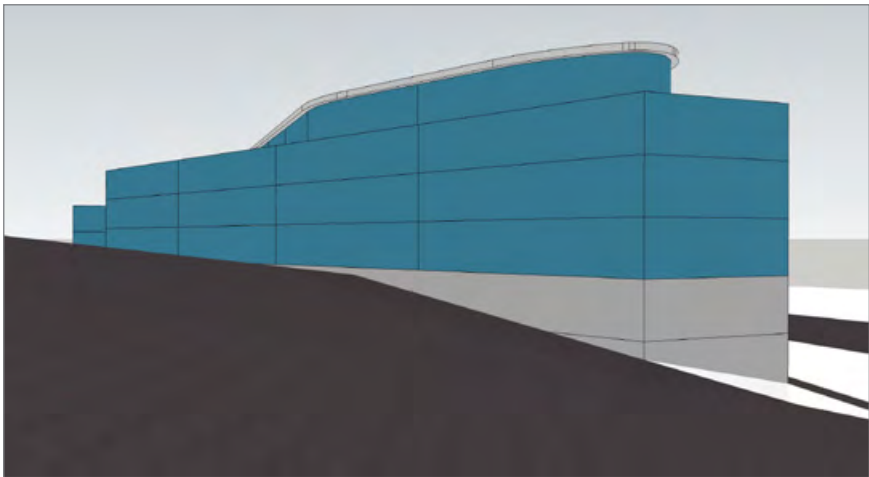


Figure 25 view from queen street and trinca lane intersection of the corner feature form

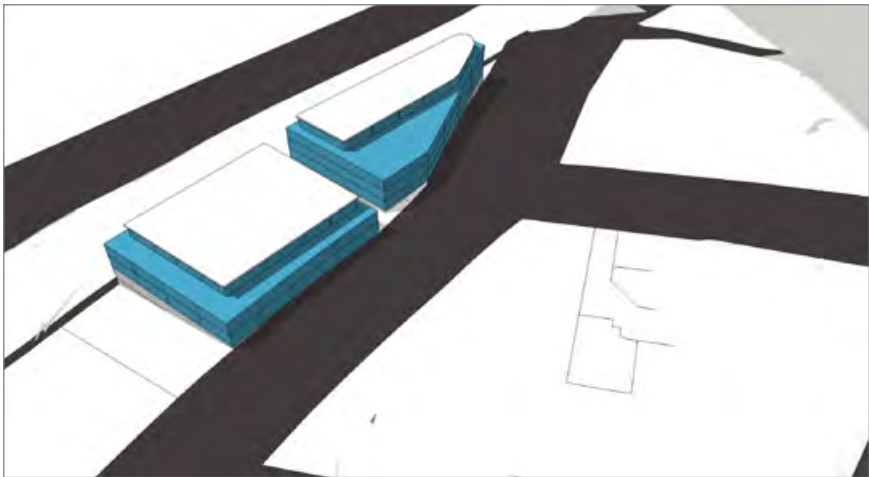
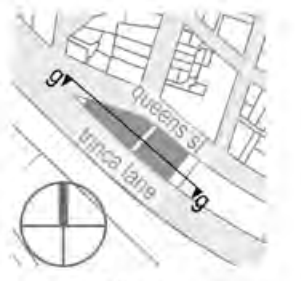


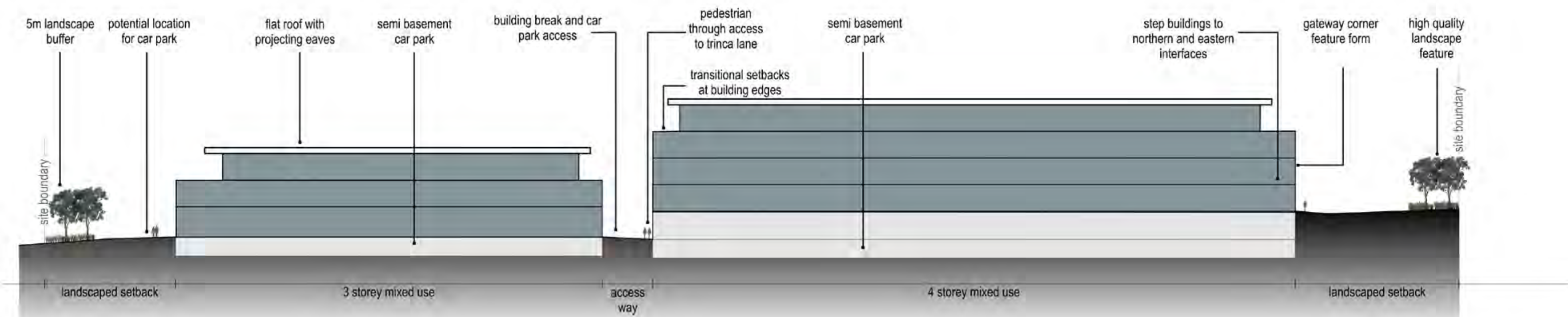
Figure 26 aerial view of gateway buildings from queen street (east).



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warragul town centre
master plan
western gateway section



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10 traffic and transport assessment

Cardno Grogan Richards have been engaged by Hansen Partnership on behalf of Baw Baw Shire Council to develop a Traffic and Transport Plan for the Warragul Town Centre (WTC) as part of the Warragul Town Centre Master Plan. The following is a summary of Cardno Grogan Richard's Warragul Town Centre Master Plan: Traffic and Transport Assessment (October 2010).

The scope of tasks undertaken by Cardno Grogan Richards included:

- Review all relevant Council reports previously undertaken in Warragul;
- Assess and confirm the function of the road hierarchy described in the Warragul Town Centre Urban Design Framework;
- Determine the public transport infrastructure requirements associated with the current and future public transport servicing the town centre;
- Describe the traffic access arrangements for the master planned areas;
- Assess car parking ratios in the town centre to ensure that they are consistent with the delivery of the town centre vision;
- Develop a car parking strategy addressing the short (1-2 years), medium (3-10 years) and long term (10+ years) parking requirements for the WTC; and
- Determine proposed transport projects (including preliminary designs) to improve traffic flows, pedestrian or cycle access within the study area.

The key conclusions and recommendations for the Warragul Town Centre Master Plan are:

- Based on the investigations and analysis summarised in the report it is concluded that: Preliminary analysis of the proposal in regards to the reconfiguration of Smith Street / Victoria Street indicates the surrounding road network can continue to operate satisfactorily;
- It is suggested a detector loop be installed at the stop line located along Smith Street at the Smith Street / Victoria Street intersection to facilitate traffic flow and reduce any delays or queues that may arise from the proposal;

- Swept path diagrams indicate that a bus can negotiate the roundabouts at Victoria Street / Palmerston and Palmerston Street / Smith Street;
- The study area covered by the 2010 surveys in the Warragul Town Centre (WTC) is more expansive than previous studies which focused on the CBD core. Therefore for comparative purposes, survey data outside of the 2003 survey area, has been excluded from analysis when making comparisons. Peak parking demand for the WTC 2010 survey area occurred at 11:00am on the Friday (7th May 2010) with 2458 spaces occupied out of the 4082 spaces surveyed, equivalent to an occupancy level of 60%. However, analysing the data for the 2003 study area alone, parking occupancy was at 72%. Narrowing the survey area further would produce higher occupancy rates accordingly.
- The majority of on-street and off-street car parking spaces located within the 'heart' of the WTC were fully occupied at times throughout the survey with occupancy rates exceeding 90%. High levels of occupancy were recorded in many short term spaces which have convenient access to shops and cafés and these spaces typically have a high turnover of vehicles due to the short time restrictions in place.
- A shortage of conveniently located short and long term parking is apparent in the Warragul Town Centre. Additional parking supply is needed to address growth within Warragul. While car parking could be provided in the future on the 3 key precincts, consideration needs to be given to providing additional car parking areas in the short to medium term. The low cost options (subject to agreement from landowners) would be to develop at grade car parking on existing vacant land on the Butter Factory precinct or on the site on the north-west corner of Mason Street and Williams Street. The more expensive option given Council's current ownership of car parks within the Warragul Town Centre would be to investigate options for a multi level car park on the Barkly Street car park site or another site close to the retail heart.

- Following the trial of pedestrian crossings in Smith Street it is recommended that they be installed at the following locations:
 - South of Barkly Street
 - Midblock between Palmerston Street and Victoria Street
 - North of Victoria Street
- As part of the redevelopment of the Butter Factory Precinct it is recommended that a new signalised intersection be installed at the Mason Street and Palmerston Street intersection to provide vehicular and pedestrian connections into the site.
- Consideration should be given to the installation of an on-road bicycle lane on Mason Street to run between Albert Street and Queen Street with the redevelopment of the Butter Factory Precinct and the Williams Square Precinct.
- A review of bus services in Warragul has been undertaken by GHD on behalf of the Department of Transport, the release of which is subject to the Minister of Transport's approval. The locations of bus stops within the WTC should remain unchanged for the foreseeable future, at least until the above mentioned report is released.

11 implementation

The implementation of the master plan will rely on a range of mechanisms including capital works funding, planning scheme amendments and coordination of actions by an overarching organisation such as Council.

timing/staging

The projects identified in the Warragul Town Centre Masterplan will be delivered in staged approach in consideration of budgetary implications and Council priorities.

funding

In order to facilitate the actions of the master plan it is envisaged part funding could be sought from a number of different State Government Departments. These may include the Department of Transport, Department of Planning and Community Development, Department of Innovation, Industry and Regional Development. In addition to the State Government Departments there may be limited Federal Government Departments where grant applications could be submitted.

planning mechanisms

To provide this master plan with an element of statutory weight, it is recommended that council moves to formally adopt the master plan. If Council supports the concepts and actions outlined in this master plan, a planning scheme amendment should be prepared and formally exhibited alongside the Warragul Town Centre Urban Design Framework and Station Precinct master plan through the Baw Baw Shire Planning Scheme.

It is recommended that the following planning mechanisms be utilised to implement the masterplan:

- amend the Municipal Strategic Statement to include the master plan as a reference document
- introduce a design and development overlay for the development sites, properties adjoining Trinca Lane, the gateway sites and the Albert & Gladstone Streets precinct
- consider the relevance or appropriateness of including the development sites in a Development Plan Overlay
- Rezoning Civic Park from Public Use Zone 6 to Public Park and Recreation Zone

To ensure the orderly development of the key development sites the Council has a number of options for introducing planning policies which provide the statutory weight to give effect to the guidelines incorporated into the master plan. These are discussed below:

A local planning policy can include performance based requirements for both built form and land use. A local planning policy only has effect when the trigger for permit is contained in another provision of the planning scheme. The policy outcomes can only be discretionary.

A Design and Development Overlay (DDO) schedule addresses only built form outcomes, not land use directly. The schedule to a DDO can set the planning permit requirements and 'trigger' the need for a planning permit. A DDO has greater statutory weight for this reason. Introducing a DDO would complement the requirements of the land use zone for a site.

A Development Plan Overlay (DPO) can address both built form and land use requirements and acts as the trigger for the preparation of a development plan prior to an application for planning permit. For sites with multiple owners, it can act as a trigger for collaboration. Alternatively, it can also be of assistance where it is anticipated that development will be staged. A development plan endorsed under the DPO has statutory weight and formalises the realisation of site master plans in an orderly manner. If a DPO was applied to the development sites, then it would be up to the land owners to formally prepare and submit a plan to the Council for approval prior to seeking planning permits for their respective parcels of land. A DPO can also remove the right for third party notice and review.

It is recommended that Council consider applying the Design and Development Overlay as the preferred mode as it provides an appropriate degree of flexibility for performance based assessment. If a DPO was preferred by Council given its ability to coordinate development of large sites, then the DDO would not be necessary as well.

place management

The implementation of the town centre masterplan would benefit from a place management approach. "Place Management" is advocated by the Departments of Planning and Community Development (DPCD) as useful mechanism for implementing activity centre visions. DPCD's Place Management Information Booklet (Sep 2008) provides a detailed outline of the place management approach and a guide to establishing a place management program.

implementation program

The program for implementing the master plan is detailed overleaf for the plans which seek to implement capital works.

It is difficult to specify timelines and responsibilities for private investment which will occur in the precincts including the Development Sites, Trinca Lane, Albert & Gladstone Streets, and Gateway Sites.

There is no imperative for development of one precinct to follow another as the Urban Design Framework and Master Plan seek to provide the necessary guidance for how private investment should occur. However, the planning mechanism available to Council should be implemented in the short term to ensure that when development occurs, the necessary planning tools are in place to guide decision making.

implementation table

project	associated tasks	timeframe
Smith Street 'Retail Heart' improvements	Concept & detailed design Prepare working drawings Call tenders Construct works	Medium-term
Palmerston Street 'Retail Heart' improvements	Concept & detailed design Prepare working drawings Call tenders Construct works	Short-term
Smith Street 'Green Spine' Improvements'	Concept & detailed design Prepare working drawings Call tenders Construct works	Medium-term
Trinca Lane improvements	Concept & detailed design Prepare working drawings Call tenders Construct works	Medium term
Town centre laneway improvements	Concept & detailed design	Medium term
Civic precinct	Concept & detailed design	Medium term

12 appendices

appendix 1: probable costs

Smith Street Green Spine (Between Albert Street & Palmerston Street)	
DESCRIPTION	AMOUNT
Demolition	\$2,500.00
Roadworks	\$5,000.00
Paving	\$9,500.00
Furniture	\$20,000.00
Street lighting	\$180,000.00
Softworks	\$90,500.00
TOTAL	
Sub Total	\$307,500.00
Contingency 15%	\$46,125.00
TOTAL EXCLUDING GST	\$353,625.00

Smith Street (between Victoria Street and Palmerston Street)	
DESCRIPTION	AMOUNT
Demolition	\$51,000.00
Roadworks	\$12,000.00
Paving	\$376,000.00
Furniture	\$48,500.00
Street lighting	\$105,000.00
Softworks	\$85,500.00
Sub Total	\$678,000.00
Contingency 15%	\$101,700.00
TOTAL EXCLUDING GST	\$779,700.00

Palmerston Street (Between Victoria Street and Smith Street)	
DESCRIPTION	AMOUNT
Demolition	\$51,000.00
Roadworks	\$18,500.00
Paving	\$265,000.00
Street lighting	\$75,000.00
Structures	\$190,000.00
Softworks	\$19,000.00
TOTAL	
Sub Total	\$618,500.00
Contingency 15%	\$92,775.00
TOTAL EXCLUDING GST	\$711,275.00

